christian@c615.co

LANDSCAPE ARCHITECT/PLANNER:

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CIVIL ENGINEER:

ARCHITECTS:

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PROPERTY INFORMATION COUNCIL DISTRICT: (8) NANCY VANREECE

Parcel ID Acreage 288 Broadmoor Dr 06100001300 15.4 ac Paro South, LLC 06100001400 1.6 ac 292 Broadmoor Dr Paro South, LLC 06100001500 2 ac 296 Broadmoor Dr Paro South, LLC 06100008500 22.3 ac BEN ALLEN LAND PARTNERS, LLC 329 Ben Allen Rd BEN ALLEN LAND PARTNERS, LLC 341 Ben Allen Rd 06100001800 8.6 ac 349 Ben Allen Rd 06100007300 3 ac BEN ALLEN LAND PARTNERS, LLC

TOTAL SITE ACREAGE

CURRENT ZONING: All properties are zoned RS10.

US FEMA FIRM NUMBER: 47037C0207F. EFFECTIVE ON 4/20/2001

R.O.W. DEDICATION AREA: No public roads. Any required dedication to meet MCSP will be finalized in the Final SP upon detailed survey.

COMMON OPEN SPACE: 16.7 ac

the south to serve the neighborhood.

Approx. 12.60 acres of Community Open Space and Forest Approx. 1.0 acre of Community Lawn

Approx. 2.0 acres of Community Garden and Orchard Approx. 1.1 acre (4000 l.f.) of Primary Trail (Greenway) Easement

PURPOSE

INFILL DEVELOPMENT NODE WHICH SUPPORTS A VARIETY OF RESIDENTIAL USES WITHIN THE VEHICULAR CIRCULATION NETWORK OF THIS DISTRICT.

GENERAL PLAN / COMMUNITY PLAN CONSISTENCY:

The Broadmoor and Ben Allen SP is a TRAIL ORIENTED DEVELOPMENT consistent with the

THE PURPOSE OF THIS PLAN IS TO PROVIDE A TRAIL-ORIENTED DEVELOPMENT MIXED USE

following policies within the Nashville Next Community Plan: T3-NE (Neighborhood Evolving) Γ3-NC (Suburban Neighborhood Center) The plan is consistent with the policies of the East Nashville Community Plan Amendment

The development indicates neighborhood centers with mixed use development and with a mix of residential housing choices while oriented around pedestrian and bicycle connectivity, dedicated open space and providing a concentrated walkable development nodes along the arterial street to the north (Broadmoor) and the local street, Ben Allen, to

The residential portions of the site are policy T3- NE and indicate a variety of housing types with sensitivity to the identified stream, drainageways and grades while oriented around pedestrian and bicycle connectivity via a primary trail and private trail systems and OVERALL DEVELOPMENT AND PHASE DESIGNATIONS:

OVERALL DEVELOPMENT:

NEIGHBORHOOD CENTERS (BROADMOOR AND BEN ALLEN): 63,000 S.F. MAX. MIXED USE/COMMERCIAL USES (Refer to LAND USE TABLE for range of accepted uses)

Fall back zoning for Neighborhood Centers is MUN-A

OVERALL NEIGHBORHOOD CENTERS PARKING: 275 Parking Spaces Required Existing Uses to Remain within Neighborhood Center: 30 spaces total

Light Manufacturing / 12K S.F. @ 1/1500 s.f. = 8 spaces Multi-Media Production: / 16.8K S.F. @ 1/750 s.f. = 22 spaces Mixed Use/Commercial: 42k s.f. (Based on 65% gen retail/35% restaurant and 20 room

hotel)* = 235 spaces required* • Camp and Support / 10 platforms maximum with 3k s.f. support = 10 spaces required

* determination of land use mix for NEIGHBORHOOD CENTERS shall be submitted with Final SP. Shared parking to be provided in NEIGHBORHOOD CENTERS based on mix of uses has not yet been calculated.

OVERALL RESIDENTIAL UNITS**: 321 TOTAL MAXIMUM Apartments, 101 maximum units

Cottages: 100 maximum units

Townhomes: 52 maximum units Hillside Duplex: 48 maximum units

Detached Dwelling w Accessory Unit (2 units each structure): 20 maximum units

OVERALL RESIDENTIAL PARKING**: 541 Parking Spaces Required Apartments, 63 units / 10% 2bdrm, 90% 1bdrm = 69 bdrm / 69 spaces req'd

Cottages: 96 maximum units / 2 spaces per unit / **192 spaces required** Townhomes, 52 maximum units / 2 spaces per unit / 104 spaces required Hillside Duplex: 50 maximum units / 2 spaces per unit / 100 spaces required Detached Dwelling w Accessory Unit: 10 dwellings plus 1 accessory each / 2 spaces per dwelling plus 1 / acc. unit / 30 spaces required ** estimated unit mix. Determination of unit mix shall be submitted with Final SP.

OVERALL MAXIMUM IMPERVIOUS SURFACE RATIO: .60

TOTAL PARKING: Required Provided

Phase 1 - 248 313 (38 spaces will be applied to Phase 3) Phase 2 – 108 118 Phase 3 – 96 74 (plus 38 spaces provided within Phase 1)

Phase 4 - 84 <u>Phase 5 - 278 317</u>

PHASE 1/BROADMOOR NEIGHBORHOOD CENTER: approximately 12.6 ACRES (SP DEVELOPMENT FALL BACK STANDARD FOR PHASE 1 IS MUL-A) BUILDING FRONTAGE BROADMOOR: 50% MINIMUM

FLOOR AREA RATIO: .80 MAXIMUM FAR Uses Allowed: REFER TO LAND USE TABLE AND EXISTING USES TO REMAIN

Build To Zone for new structures/ Broadmoor: 5-20'* Side Setback: 0' Rear Setback: 0'

Height Maximum: 3 stories

Height Minimum. 14' one story

PARKING REQUIRED: 248 spaces required (not including shared parking reductions) PARKING PROVIDED: 313 spaces provided * Parking to be provided per zoning code with shared parking available with Phase 1 /Neighborhood Center

**outdoor dining areas are allowed to be zero setback provided that public sidewalk meeting MCSP is provided in adjacent public ROW. Build-to lines are subject to proximity of overhead power lines.

EXISTING STRUCTURE A and B: Existing Structures to Remain / 30k s.f.

Uses Allowed: REFER TO LAND USE TABLE AND EXISTING USES TO REMAIN Parking: Provided per zoning code with shared parking available with Phase 1 uses / 30 spaces required EX. STRUCTURE A – Light Manufacturing / 12K S.F. @ 1/1500 s.f. = 8 spaces EX. STRUCTURE B - Multi-Media Production : / 16.8K S.F. @ 1/750 s.f. = 22 spaces

LIVE/WORK (MAKE) BUILDING A

Approx. 9,500 s.f. of live/work or live/make with commercial and/or residential uses (7 units) Uses Allowed: REFER TO LAND USE TABLE AND EXISTING USES TO REMAIN Parking: Provided per zoning code with shared parking available with Phase 1 uses / Parking required 7

MIXED USE BUILDING A:

Approx. 8,000 s.f./ 3 stories maximum MIXED USE Uses Allowed: REFER TO LAND USE TABLE

Parking: Provided per zoning code with shared parking available with Phase 1 uses / Parking required 50

MAR ACRES

Approx. 21,100 s.f./ 3 stories w ground floor commercial and upper floors of residential (20 units) Parking: Provided per zoning code with shared parking available with Phase 1 uses / Parking required 49

RESIDENTIAL (attached Townhomes): 52 dwelling units maximum Uses Allowed: REFER TO LAND USE TABLE

Private drive setback: 5' minimum edge of drive

Side Setback: 0'

Rear Setback: 0' Height Maximum: 3 stories and 35' along private drive face including basement/carport level to Parking: Provided per zoning code within attached garages or immediately adjacent surface parking / 2 spaces per unit / 104 spaces required

PHASE 2: approximately 6.4 ACRES (SP DEVELOPMENT FALL BACK STANDARD FOR PHASE 2 IS RM15)

RESIDENTIAL (detached Cottages): 54 dwelling units (various sizes)

Uses allowed: Refer to LAND USE TABLE **Building Mass:**

Private drive setback: 5' minimum from edge of drive

Side Setback: 6' minimum between structures

Rear Setback: not applicable Height Maximum, 2 stories and 35' along private drive face a third exposed basement level shall be allowed as needed, to accommodate site grade change. Parking: Provided per zoning code within attached garages or immediately adjacent surface parking / 2 spaces per unit / 108 spaces required

PHASE 3: approximately 10.4 ACRES (SP DEVELOPMENT FALL BACK STANDARD FOR PHASE 3 IS RM15)

RESIDENTIAL (Hillside Duplex): 48 dwelling units maximum / maximum of 2 units attached

Uses allowed: : REFER TO LAND USE TABLE **Building Mass** Private drive setback: 5' minimum from edge of drive Side Setback: 6' minimum between structures

Rear Setback: 0' Height Maximum: 3 stories and 35' along private drive face including basement/carport level to accommodate site grade change. Parking: Provided per zoning code with surface parking or park under carport/ 2 spaces per unit / 100 spaces required

PHASE 4: approximately 6.9 ACRES (SP DEVELOPMENT FALL BACK STANDARD FOR PHASE 4 IS RM15)

RESIDENTIAL (detached Cottages): 27 dwelling units (various sizes) Uses allowed: Refer to LAND USE TABLE

Side Setback: 6' minimum between structures Rear Setback: not applicable Height Maximum, 2 stories and 35' along private drive face. A third exposed basement

level shall be allowed, as needed, to accommodate site grade change. Parking: Provided per zoning code within attached garages or immediately adjacent surface parking / 2 spaces per unit / 54 spaces required

Private drive setback: 10' minimum from edge of drive

FLOOR AREA RATIO: .80 MAXIMUM FAR Uses Allowed: REFER TO LAND USE TABLE AND EXISTING USES TO REMAIN **Building Mass:**

Front Setback/ Ben Allen 5' minimum Side Setback: 0' Rear Setback: 0' Height Maximum: 3 stories

PARKING PROVIDED: 317 spaces provided 3 Parking to be provided per zoning code with shared parking available with Phase 1 /Neighborhood Center uses **outdoor dining areas are allowed to be zero setback provided that public sidewalk

PARKING REQUIRED: 278 spaces required (not including shared parking reductions)

meeting MCSP is provided in adjacent public ROW. Build-to lines are subject to proximity of overhead power lines.

LIVE/WORK (MAKE) BUILDING B: Approx. 9,600 s.f. total of live/work or live/make with commercial and residential uses Uses Allowed: REFER TO LAND USE TABLE

Building Mass: Front Setback/ 5' minimum from back of private sidewalk Side Setback: 0 Rear Setback: 0' Height Maximum: 3 stories Height Minimum. 12' first floor

Parking: Provided per zoning code with shared parking available with Phase 1 uses / Parking required 14 spaces MULTIFAMILY BUILDING A and B:

Approx. 15,700 s.f. total of residential uses (38 units) Uses Allowed: REFER TO LAND USE TABLE Building Mass: Front Setback/ 5' minimum from back of private sidewalk Side Setback: 0' Rear Setback: 0'

Height Maximum: 3 stories

Height Minimum, 12' first floor Parking: Provided per zoning code with shared parking available with Phase 5 uses / Parking required 76 spaces

MIXED USE BUILDINGS C and D: Approx. 25,000 s.f. total/w approx.. 8500 s.f. commercial and remaining residential/mixed use (24 units) Uses Allowed: REFER TO LAND USE TABLE

Building Mass: Front Setback/ 5' minimum from back of private sidewalk Side Setback: 0' Rear Setback: 0' Height Maximum: 3 stories

Height Minimum. 14' first floor Parking: Provided per zoning code with shared parking available with Phase 5 uses / Parking required 70 spaces

MIXED USE BUILDINGS E, F, G AND H: Approx. 22,000 s.f.total /. w approx. 9,000 s.f. commercial / mixed use including camping support services and mixed use/hotel (20 keys) with alternate use of residential Uses Allowed: REFER TO LAND USE TABLE

Building Mass: Front Setback/ 5' minimum from back of Ben Allen or private sidewalk Side Setback: 0' Rear Setback: 0'

Height Maximum: 3 stories Height Minimum. 14' first floor Parking: Provided per zoning code with shared parking available with Phase 5 uses / Parking required 97 spaces

RESIDENTIAL (detached Cottages) 19 dwelling units (various sizes) Uses allowed: Refer to LAND USE TABLE **Building Mass:**

Private drive setback: 5' minimum from edge of drive Side Setback: 6' minimum between structures Rear Setback: not applicable

Height Maximum, 2 stories and 35' along private drive face. A third exposed basement level shall be allowed, as needed, to accommodate site grade change. Parking: Provided per zoning code within attached garages or immediately adjacent surface parking / 2 spaces per unit / 36 spaces required

BEN ALLEN MIXED USE AREA W/N

2970' OF PROPOSED SIDEWALK

BEN ALLEN EX. SIDEWALK IN ROW

SIDEWALK EXISTS. PROPOSE TO IMPROVE BY ADDING PLANTING

BEN ALLEN PROP. PRIMARY TRAIL IN ROW

PROPOSE TO ADD MULTIUSE PATH TO

NO SIDEWALK EXISTS.

CONNECT TO GREENWAY

DEVELOPMENT SCHEDULE

Phasing is currently anticipated to be five phases as noted.

A finite timeline for phasing will be determined consistent with leasing and financing.

LAND USE and MODIFICATIONS

Refer to LAND USE TABLE for Land Uses Appropriate for the entire site and for those uses appropriate for Neighborhood Centers

ENCROACHMENTS EXPOSED, COVERED, OR ENCLOSED PORCHES AND DECKS MAY EXTEND INTO FRONT SETBACKS ALONG PUBLIC ROADS OR COMMON SPACE NOT EXCEEDING 6FT INTO COMMON SPACE OR 3FT ALONG PUBLIC ROADS.

REFER TO MUNICODE 17.12.040: OTHER SETBACKS FOR ALL OTHER ALLOWABLE ENCROACHMENTS NOT IDENTIFIED ABOVE.

DETAILED DESIGN. FINAL DETAILS OF THIS PLAN SHALL BE GOVERNED BY THE

APPROPRIATE REGULATIONS AND REQUIREMENTS OF THE FINAL SP APPLICATION.

PARKING THIS DRAWING INDICATES THE BASIC DEVELOPMENT PLAN FOR THE SITE. THE PARKING LAYOUTS SHOWN ARE REPRESENTATIVE AND ARE SUBJECT TO CHANGE FOLLOWING

PARKING SHALL MEET METRO ZONING ORDINANCE FOR EACH USE. SHARED PARKING SHALL BE INCORPORATED FOR NEIGHBORHOOD CENTERS.

LANDSCAPE THE DEVELOPMENT OF THIS PROJECT SHALL **EXCEED** THE REQUIREMENTS OF METRO ZONING CODE CHAPTER 17.24 ARTICLE II: TREE PROTECTION AND REPLACEMENT BY A

MINIMUM 20%, AND SHALL MEET REQUIREMENTS OF CHAPTER 17.40 ARTICLE X: TREE PROTECTION AND REPLACEMENT PROCEDURES. DETAILED LANDSCAPE ORDINANCE PLAN TO BE SUBMITTED WITH THE FINAL SP SUBMITTAL. NO LANDSCAPE BUFFERS ARE PROVIDED.

THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE, AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY. THIS IS IN ACCORDANCE WITH THE STORMWATER MANAGEMENT MANUAL VOLUME 1 REGULATIONS.

SOIL CLASSIFICATIONS

ROADWAY CLASSIFICATIONS

BROADMOOR DRIVE: T4-R-CA2 (T4 Urban Residential Collector-Avenue) BEN ALLEN ROAD: Local Road

REFER TO TRAFFIC IMPACT STUDY, PREPARED BY RPM TRANSPORTATION, LLC AND DATED

GENERAL NOTES

EXISTING COMMERCIAL STRUCTURES AND USES WITHIN BROADMOOR NEIGHBORHOOD CENTER TO REMAIN.

ALL DEVELOPMENT WITHIN THE BOUNDARIES OF THIS PLAN MEETS THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT AND FAIR HOUSING ACT.

THIS PROPERTY DOES NOT LIE WITHIN AN AREA DESIGNATED AS SPECIAL FLOOD HAZARD AREA ACCORDING TO FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE MAPS. (REFER TO CIVIL PLAN)

TRASH AND RECYCLING SERVICES TO BE CONTRACTED BETWEEN THE DEVELOPER/OWNER AND A PRIVATE HAULER FOR ALL UNITS ON PRIVATE DRIVES. USPS MAIL DELIVERY WILL BE PROVIDED BY CENTRALIZED KIOSKS/MAIL ROOMS WITHIN

SIDEWALKS AND TRAILS

SPECIFICATIONS

BROADMOOR / BEN ALLEN SP

SIDEWALK STUDY

2017-06-13

THE FINAL SITE PLAN/ BUILDING PERMIT SITE PLAN SHALL DEPICT THE REQUIRED PUBLIC SIDEWALKS, ANY REQUIRED GRASS STRIP OR FRONTAGE ZONE AND THE LOCATION OF ALL EXISTING AND PROPOSED VERTICAL OBSTRUCTIONS WITHIN THE REQUIRED SIDEWALK AND GRASS STRIP OR FRONTAGE ZONE. PRIOR TO THE ISSUANCE OF USE AND OCCUPANCY PERMITS, EXISTING VERTICAL OBSTRUCTIONS SHALL BE RELOCATED OUTSIDE OF THE REQUIRED SIDEWALK. VERTICAL OBSTRUCTIONS ARE ONLY PERMITTED WITHIN THE REQUIRED GRASS STRIP OR FRONTAGE ZONE.

PRIMARY TRAIL SYSTEM SHALL BE CONSTRUCTED TO MEET THE OUTDOOR AMERICAN WITH DISIBILITIES ACT ACCESSIBILITY GUIDELINES AND WILL BE PROVIDED AT A MINIMUM WIDTH OF 12' AND SHALL BE CONSTRUCTED TO MEET METRO PARKS AND GREENWAYS STANDARDS AS EACH PHASE OF DEVELOPMENT IS COMPLETED. PRIMARY TRAIL WILL BE

ALLEN ROAD SHALL BE 5.5' IN WIDTH AND INCLUDE A 5' PLANTING STRIP UNLESS

INDICATED AS AN EXTENSION OF THE "PRIMARY TRAIL SYSTEM".

SIDEWALKS ALONG BROADMOOR DRIVE SHALL MEET MCSP. THE SIDEWALK ALONG BEN

LIT MEETING CRITERIA FOR DARK SKY LIGHTING OR OTHER LOW INTENSITY TECHNIQUES TRAIL AMENITIES ALONG THE PRIMARY TRAIL WILL INCLUDE AT A MINIMUM: A TRAILHEAD NEAR EACH END OF THE PRIMARY TRAIL PET WASTE DISPENSERS A MINIMUM OF ONE PER 1/4 MILE OF PRIMARY TRAIL TWO WATER FOUNTAINS SERVING PEOPLE AND PETS

ONE BIKE REAPIR STATION. LIGHTING FOR STREETS AND THROUGHOUT THE DEVELOPMENT WILL UTILIZE LIGHTING MEETING THE CRITERIA FOR DARK SKY COMPLIANCE OR OTHER LOW INTENSITY TECHNIQUES APPROVED BY PLANNING STAFF. ACTUAL FIXTURES/METHODS WILL BE SUBMITTED IN THE FINAL SP.

ALL CONSTRUCTION WITHIN THE ROW IS TO COMPLY WITH ADA AND MPW STANDARDS AND

STORMWATER/UTILITIES/INFRASTRUCTURE: STORMWATER PLANS FOR THIS DEVELOPMENT TO BE PROVIDED IN A MANNER CONSISTENT WITH THE STORMWATER VOLUME 5 LID MANUAL. THIS DRAWING IS FOR ILLUSTRATIVE PURPOSES TO INDICATE THE BASIC PREMISE OF THIS DEVELOPMENT AND PROPOSED STORMWATER LOCATIONS AND METHODS. FINAL DETAILS SHALL BE

GOVERNED BY THE APPROPRIATE REGULATIONS AT THE TIME OF THE SUBMITTAL SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY METRO STORMWATER

METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED ACCESS IN ORDER TO MAINTAIN AND REPAIR UTILITIES ON THE SITE.

ALL UTILITY BOXES/DEVICES LOCATED ABOVE GROUND WILL BE SCREENED

ALL PROPOSED NEW PUBLIC UTILITIES AND SERVICES SHALL BE INSTALLED UNDERGROUND. ALL ACCESS DRIVES AND DRIVEWAYS WITHIN THIS DEVELOPMENT SHALL BE PRIVATE. ALL PRIVATE DRIVES/ALLEYS MAY BE PAVED WITH PERVIOUS PAVEMENT WITH MATERIAL AND CROSS-SECTION TO BE APPROVED BY METRO PUBLIC WORKS. NO FRONT-LOADED

ALL SIDEWALKS LOCATED WITHIN THE PUBLIC R.O.W. ARE TO MEET METRO PUBLIC WORKS AND ADA STANDARDS.

ASSESSMENT PERFORMED BY BDY DATED DECEMBER 15, 2016. NO DEFINED TOP OF BANK WAS INDICATED FOR THE BEN ALLEN NEIGHBORHOOD CENTER STREAM SECTION. COPIES OF THIS STUDY ARE AVAILABLE UPON REQUEST.

BUFFER FOR ON SITE STREAMS INDICATED IN ACCORDANCE WITH AQUATIC RESOURCE

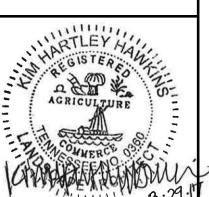
110 10th Street

Nashville, TN 37206

615.255.5218

Fax: 615.254.1424

www.hawkinspartners.com



EXP. 3/31/2017

DATE: 7-18-17

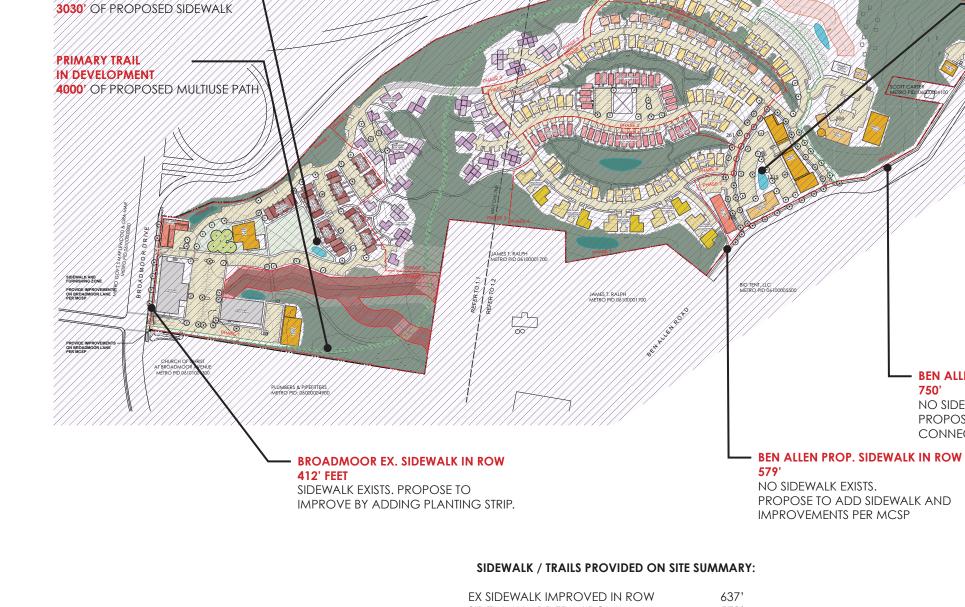
JOB NO.: 16-945

NOTES



BROADMOOR SIDEWALK ANALYSIS

SIDEWALKS TO BE PROVIDED OFFSITE (WITH COMPLETION OF PHASE 3):



SIDEWALK ADDED IN ROW SIDEWALK ADDED W/N DEVELOPMENT PRIMARY TRAIL ADDED W/N ROW PRIMARY TRAIL ADDED W/N DEVELOPMENT 4000'

SIDEWALKS / TRAILS PROVIDED ON SITE:

BROADMOOR MIXED USE AREA 🛶

W/N DEVELOPMENT

BROADMOOR / BEN ALLEN SP

SIDEWALK STUDY

2017-06-13

Building Mass Private drive setback: 5' minimum from edge of drive

RESIDENTIAL (detached Dwelling with Accessory Unit): 10 dwelling units each with an attached accessory unit (total 20 units) Uses allowed: Refer to LAND USE TABLE

Side Setback: 6' minimum between structures Rear Setback: minimum of 20' to overall parcel property line Height Maximum, 2 stories and 35' along private drive face. A third exposed basement level shall be allowed, as needed, to accommodate site grade change. Parking: Provided per zoning code within attached garages or immediately adjacent surface parking / 2 spaces per dwelling plus 1 space per accessory unit / 30 spaces

(SP DEVELOPMENT FALL BACK STANDARD FOR PHASE 5 IS MUL-A)

PHASE 5/BEN ALLEN NEIGHBORHOOD CENTER: approximately 14.6

Private drive access from Ben Allen will be provided with Phase 3 development.

MANAGEMENT MANUAL.

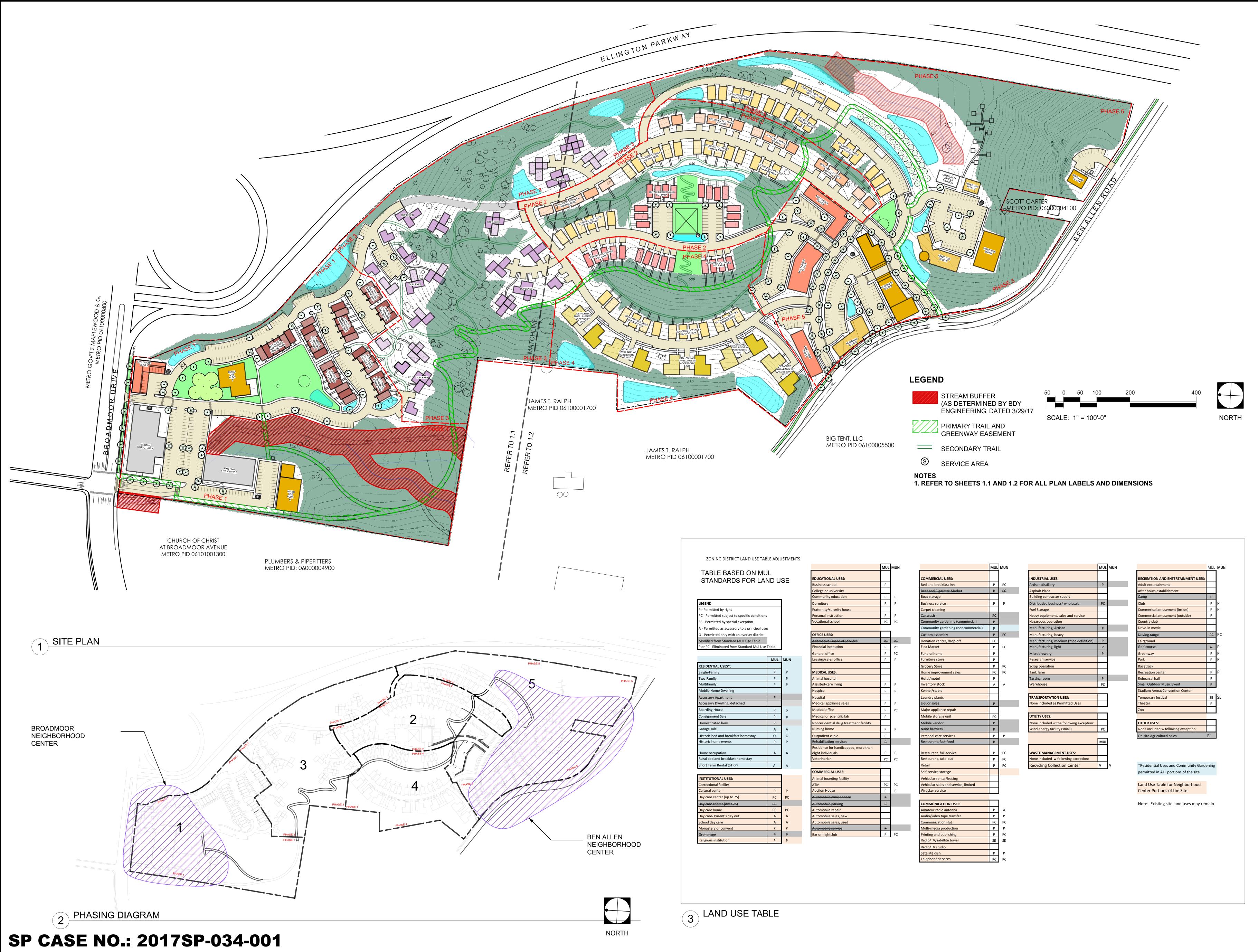
ALL WATER AND SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND STANDARD DETAILS OF METRO WATER SERVICES.

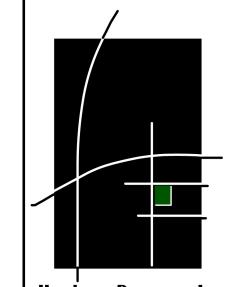
GARAGES ARE PERMITTED FRONTING ON A PUBLIC ROAD.

REFER TO CIVIL DRAWINGS FOR ALL OTHER UTILITIES/ EASEMENTS.

SP CASE NO.: 2017SP-034-001

VICINITY MAP





awkins Partners,In

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AGRICULTURE

AGRICULTURE

3.29.11

EXP. 3/31/2017

EXP. 3/31/2

EN ALLEN MASTER PLAN

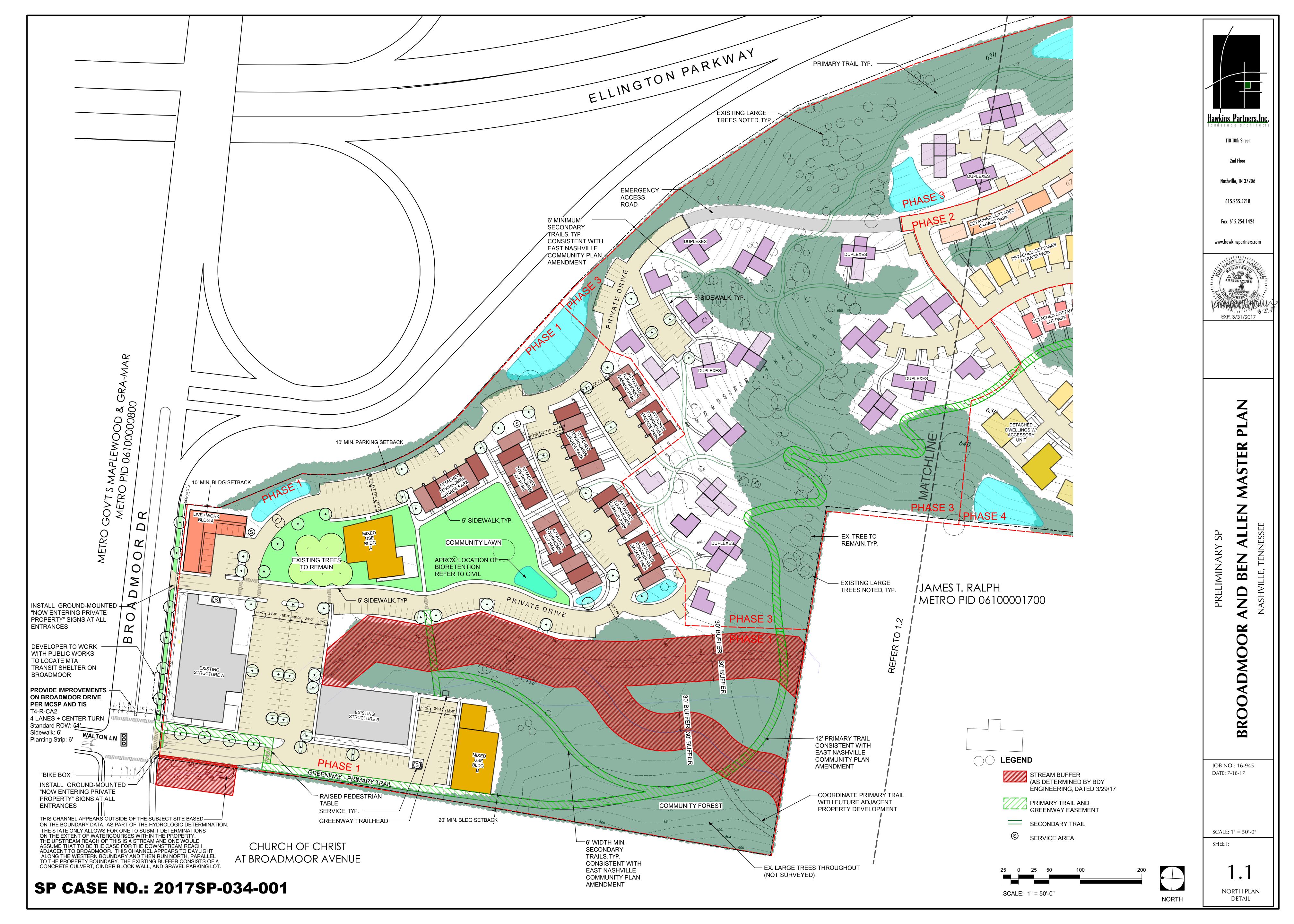
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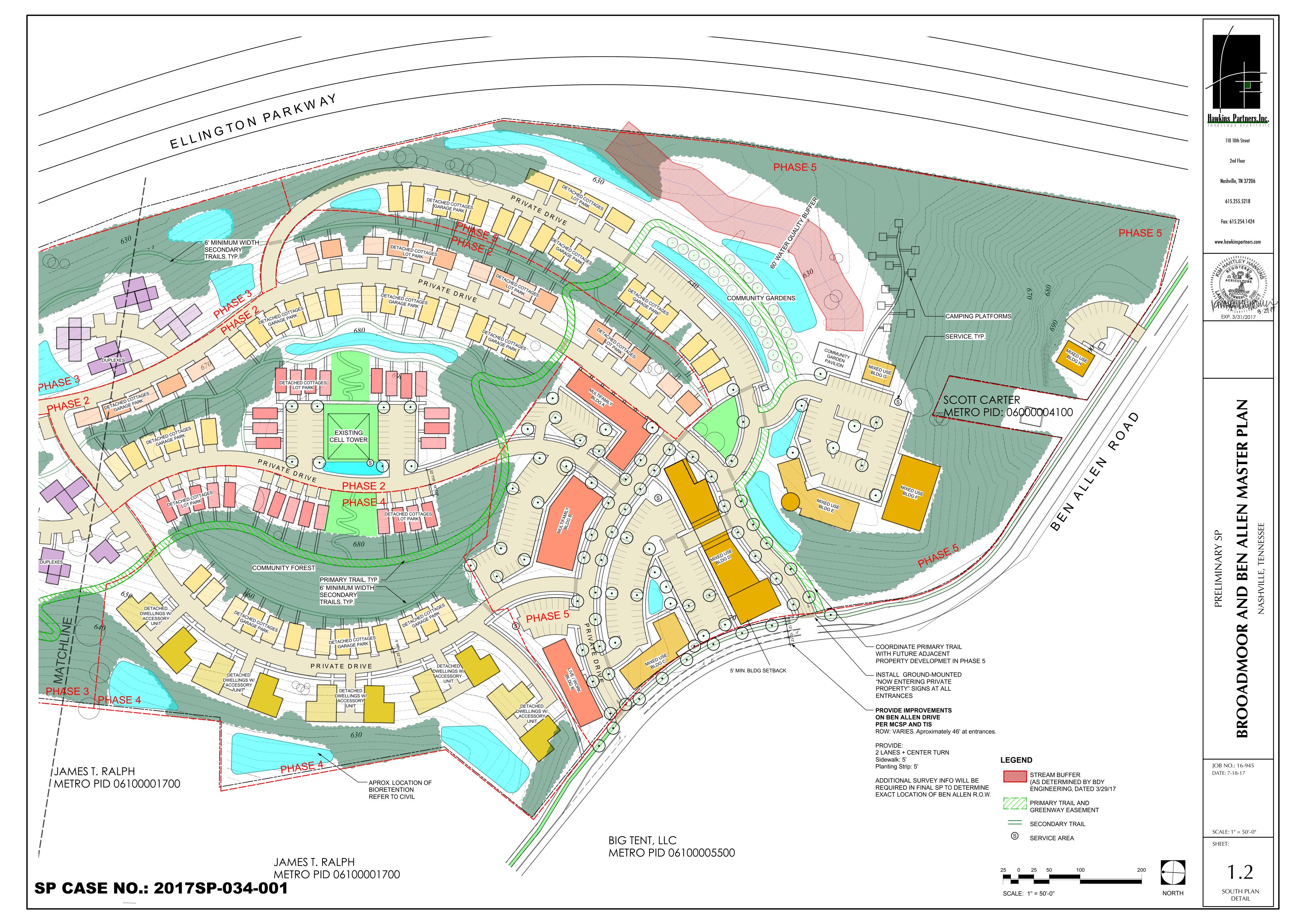
OADMOOR AND BEI

JOB NO.: 16-945 DATE: 7-18-17

SHEET:

1.0 OVERVIEW















ARCHITECTURAL REFERENCE IMAGES: NEIGHBORHOOD CENTER AT BROADMOOR







ARCHITECTURAL REFERENCE IMAGES: NEIGHBORHOOD CENTER AT BEN ALLEN







ARCHITECTURAL REFERENCE IMAGES: COTTAGES









2 ARCHITECTURAL REFERENCE IMAGES: HILLSIDE DUPLEXES





ARCHITECTURAL REFERENCE IMAGES: DETACHED DWELLINGS W/ ACCESSORY UNIT





7 ARCHITECTURAL REFERENCE IMAGES: TOWNHOMES

Character Defining Guidelines

The final building elevations shall be submitted with the final site plan and shall be consistent with the following architectural standards:

General Neighborhood Center Notes

A. Storefronts, restaurants and offices shall be activated from and front onto Broadmoor and Ben Allen B. Mixed use buildings shall provide access via glazed doors from both the front and rear of the structure.

Neighborhood Center at Broadmoor: "Maker Center"

This is the first of two mixed-use areas (One each fronting on Broadmoor and Hart Lane). This area includes the existing Studio 615 Building and an existing Steel Fabrication Building. New structures in this area are intended to complement the existing buildings to surround parking for visitors to these areas and events on The Green.

A. New construction and materials should be compatible with the existing industrial maker space/fabrication

B. Use of industrial metal panels and roofing are encouraged, although materials aren't limited to these choices. Use of EIFS, vinyl siding and aluminum clapboard siding and fake wood grain siding are prohibited

Ground floors shall include a minimum of 40% glazing; additional stories shall include a minimum of 25%

A central feature of the community, The Green is seen as a site for outdoor concerts/maker's markets/etc. It is lined with 2-over-1 townhouses that provide the ability to have maker spaces/small retail/studio spaces lining the green. The outdoor pavilion building may contain a stage/restaurant/gift shop/green rooms and restrooms.

Neighborhood Center at Ben Allen: "The Farmstead"

This second mixed-use area is more rural in character and provides areas for agricultural tourism related uses such as a small farm, farmers' market, hotel, farm to table restaurant(s), retail focused on locally made products(although not limited to this) and camping pavilions. There is also an opportunity for providing housing for seniors and younger residents in this area with the provision of micro-units and upper level residents with close access to services.

A. Wood and agricultural metal siding and metal roofs are encouraged, although materials are not limited to

Use of EIFS, vinyl siding and aluminum clapboard siding and fake wood grain siding are prohibited

Ground floors shall include a minimum of 30% glazing or open area; additional stories shall include a minimum of 25% glazing.

D. New structures are encouraged to reference agricultural buildings and farm groupings, although this is not a

Residential: Townhomes, Duplexes, Cottages and Detached Dwellings with Accessory Units Residential is included throughout. Townhomes are concentrated within phase one. While phases two, three and four contain a diverse mix of housing types suited for trail-oriented and hillside development such as duplexes, detached housing with accessory dwelling, and cottages.

Front entry garages and parking in front of dwellings are permitted as a way to work with slope.

Dwellings may have main front door facing walking trails in lieu of vehicular drives.

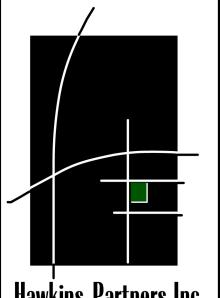
A variety of materials and building forms are encouraged. Use of EIFS, vinyl siding and aluminum clapboard siding and fake wood grain siding are prohibited

May have primary entrance on primary or secondary trails.

May be units set on piers in order to work with grade.

For Townhomes, Cottages and Detached Dwellings with Accessory Units: Building facades facing a street, provide one principal entrance and a minimum of 25% glazing.

5 ARCHITECTURAL GUIDELINES



110 10th Street

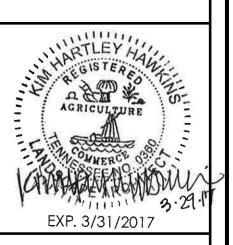
2nd Floor

Nashville, TN 37206

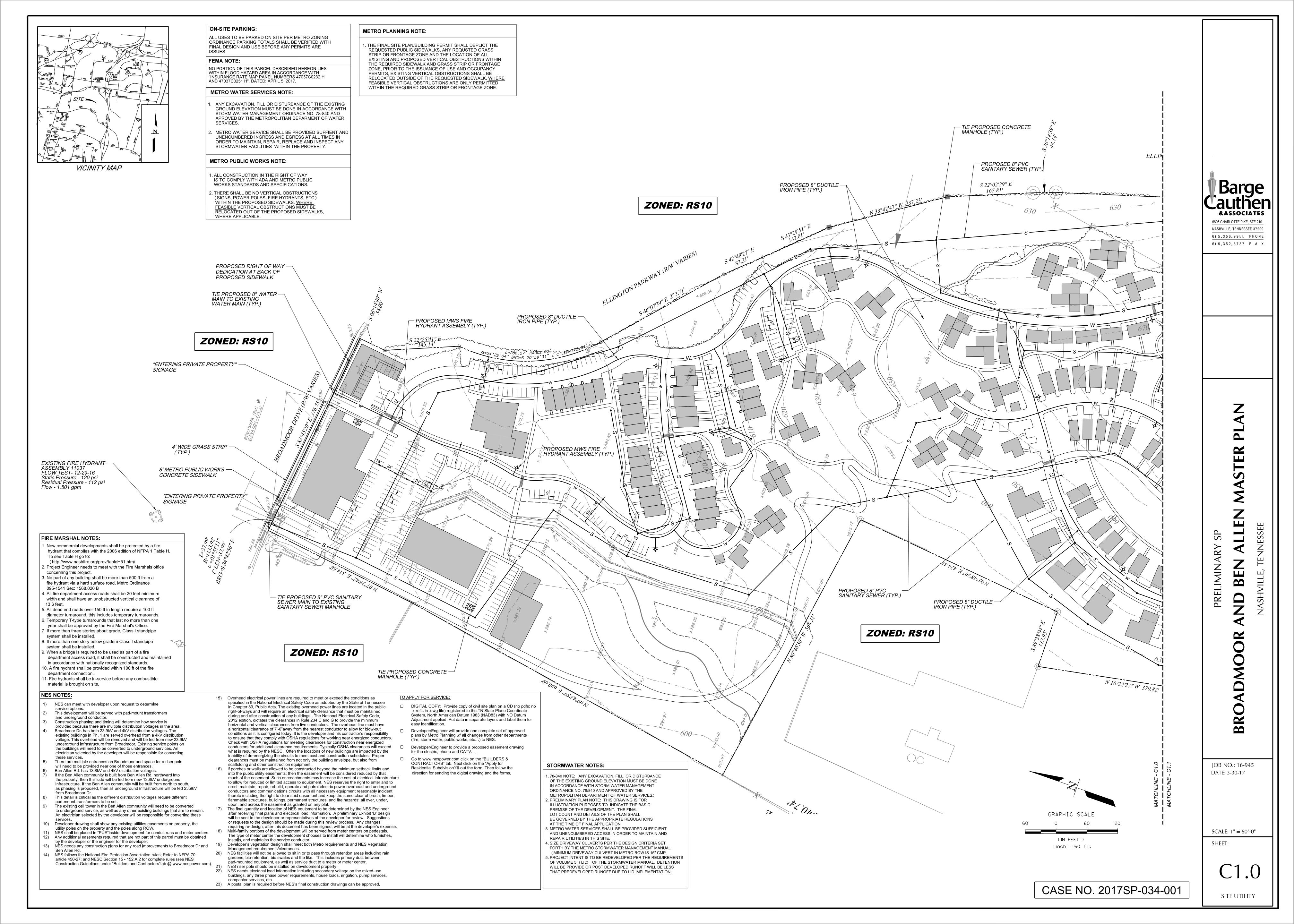
615.255.5218

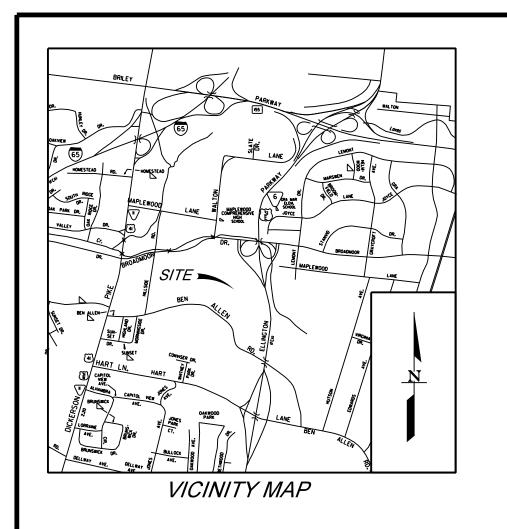
Fax: 615.254.1424

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SCALE: 1'' = 50'-0''





FIRE MARSHAL NOTES: New commercial developments shall be protected by a fire hydrant that complies with the 2006 edition of NFPA 1 Table H. To see Table H go to: (http://www.nashfire.org/prev/tableH51.htm) . Project Engineer needs to meet with the Fire Marshals office concerning this project. 3. No part of any building shall be more than 500 ft from a fire hydrant via a hard surface road. Metro Ordinance 095-1541 Sec: 1568.020 B 4. All fire department access roads shall be 20 feet minimum width and shall have an unobstructed vertical clearance of 13.6 feet. 5. All dead end roads over 150 ft in length require a 100 ft diameter turnaround, this includes temporary turnarounds. 6. Temporary T-type turnarounds that last no more than one year shall be approved by the Fire Marshal's Office. 7. If more than three stories about grade, Class I standpipe system shall be installed. 8. If more than one story below gradem Class I standpipe system shall be installed. 9. When a bridge is required to be used as part of a fire department access road, it shall be constructed and maintained in accordance with nationally recognized standards. 10. A fire hydrant shall be provided within 100 ft of the fire

NES NOTES:

department connection.

material is brought on site.

NES can meet with developer upon request to determine service options.
 This development will be served with pad-mount transformers

1. Fire hydrants shall be in-service before any combustible

and underground conductor.

Construction phasing and timing will determine how service is provided because there are multiple distribution voltages in the area. Broadmoor Dr. has both 23.9kV and 4kV distribution voltages. The

existing buildings in Ph. 1 are served overhead from a 4kV distribution voltage. This overhead will be removed and will be fed from new 23.9kV underground infrastructure from Broadmoor. Existing service points on the buildings will need to be converted to underground services. An electrician selected by the developer will be responsible for converting these services.

There are multiple entrances on Broadmoor and space for a riser pole

will need to be provided near one of those entrances.

Ben Allen Rd. has 13.8kV and 4kV distribution voltages.

If the Ben Allen community is built from Ben Allen Rd. northward into the property, then this side will be fed from new 13.8kV underground infrastructure. If the Ben Allen community will be built from north to south, as phasing is proposed, then all underground infrastructure will be fed 23.9kV

from Broadmoor Dr.
This detail is critical as the different distribution voltages require different pad-mount transformers to be set.
The existing cell tower in the Ben Allen community will need to be converted to underground service, as well as any other existing buildings that are to remain. An electrician selected by the developer will be responsible for converting these

services.

Developer drawing shall show any existing utilities easements on property, the utility poles on the property and the poles along ROW.

NES shall be placed in "PLIE" inside development for conduit runs and meter centers.

11) NES shall be placed in "PUE"inside development for conduit runs and meter centers.
12) Any additional easements required that are not part of this parcel must be obtained by the developer or the engineer for the developer.
13) NES needs any construction plans for any road improvements to Broadmoor Dr and

NES follows the National Fire Protection Association rules; Refer to NFPA 70 article 450-27; and NESC Section 15 - 152.A.2 for complete rules (see NES Construction Guidelines under "Builders and Contractors" tab @ www.nespower.com).
Overhead electrical power lines are required to meet or exceed the conditions as specified in the National Electrical Safety Code as adopted by the State of Tennessee in Chapter 89, Public Acts. The existing overhead power lines are located in the public right-of-ways and will require an electrical safety clearance that must be maintained during and after construction of any buildings. The National Electrical Safety Code, 2012 edition, dictates the clearances in Rule 234 C and G to provide the minimum horizontal and vertical clearances from live conductors. The overhead line must have a horizontal clearance of 7'-6"away from the nearest conductor to allow for blow-out conditions as it is configured today. It is the developer and his contractor's responsibility to ensure that they comply with OSHA regulations for working near energized conductors. Check with OSHA regulations for meeting clearances for construction near energized conductors for additional clearance requirements. Typically OSHA clearances will exceed

Check with OSHA regulations for meeting clearances for construction near energized conductors for additional clearance requirements. Typically OSHA clearances will exceed what is required by the NESC. Often the locations of new buildings are impacted by the inability of de-energizing the circuits to meet cost and construction schedules. Proper clearances must be maintained from not only the building envelope, but also from scaffolding and other construction equipment.

If porches or walls are allowed to be constructed beyond the minimum setback limits and into the public utility easements; then the easement will be considered reduced by that

erect, maintain, repair, rebuild, operate and patrol electric power overhead and underground conductors and communications circuits with all necessary equipment reasonably incident thereto including the right to clear said easement and keep the same clear of brush, timber, flammable structures, buildings, permanent structures, and fire hazards; all over, under, upon, and across the easement as granted on any plat.

The final quantity and location of NES equipment to be determined by the NES Engineer after receiving final plans and electrical load information. A preliminary Exhibit 'B' design will be sent to the developer or representatives of the developer for review. Suggestions

much of the easement. Such encroachments may increase the cost of electrical infrastructure to allow for reduced or limited access to equipment. NES reserves the right to enter and to

or requests to the design should be made during this review process. Any changes requiring re-design, after this document has been signed, will be at the developer's expense.

Multi-family portions of the development will be served from meter centers on pedestals. The type of meter center the development chooses to install will determine who furnishes, installs, and maintains the service conductor.

Developer's vegetation design shall meet both Metro requirements and NES Vegetation

Management requirements/clearances.

20) NES facilities will not be allowed to sit in or to pass through retention areas including rain gardens, bio-retention, bio swales and the like. This includes primary duct between pad-mounted equipment, as well as service duct to a meter or meter center.

NES riser pole should be installed on development property.

NES needs electrical load information including secondary voltage on the mixed-use buildings, any three phase power requirements, house loads, irrigation, pump services, compactor services, etc.

23) A postal plan is required before NES's final construction drawings can be approved.

TO APPLY FOR SERVICE:

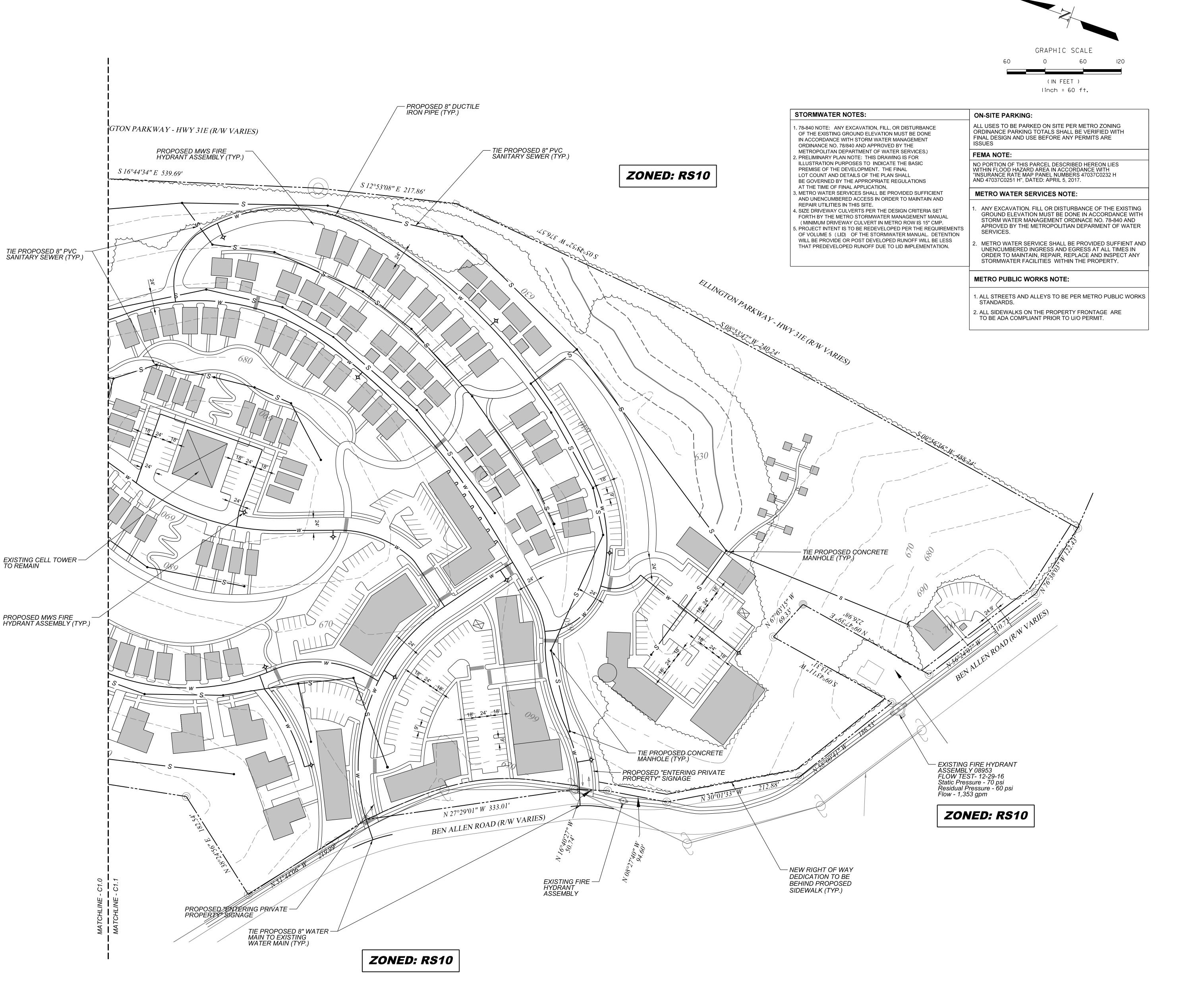
□ DIGITAL COPY: Provide copy of civil site plan on a CD (no pdfs; no x-ref's in .dwg file) registered to the TN State Plane Coordinate System, North American Datum 1983 (NAD83) with NO Datum Adjustment applied. Put data in separate layers and label them for

easy identification.

□ Developer/Engineer will provide one complete set of approved plans by Metro Planning w/ all changes from other departments (fire storm water public works etc.) to NES.

(fire, storm water, public works, etc...) to NES.□ Developer/Engineer to provide a proposed easement drawing for the electric, phone and CATV.

Go to www.nespower.com click on the "BUILDERS & CONTRACTORS" tab. Next click on the "Apply for Residential Subdivision" fill out the form. Then follow the direction for sending the digital drawing and the forms.



Barge Cauthen

6606 CHARLOTTE PIKE, STE 210

NASHVILLE, TENNESSEE 37209

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JOB NO.: 16-945 DATE: 3-30-17

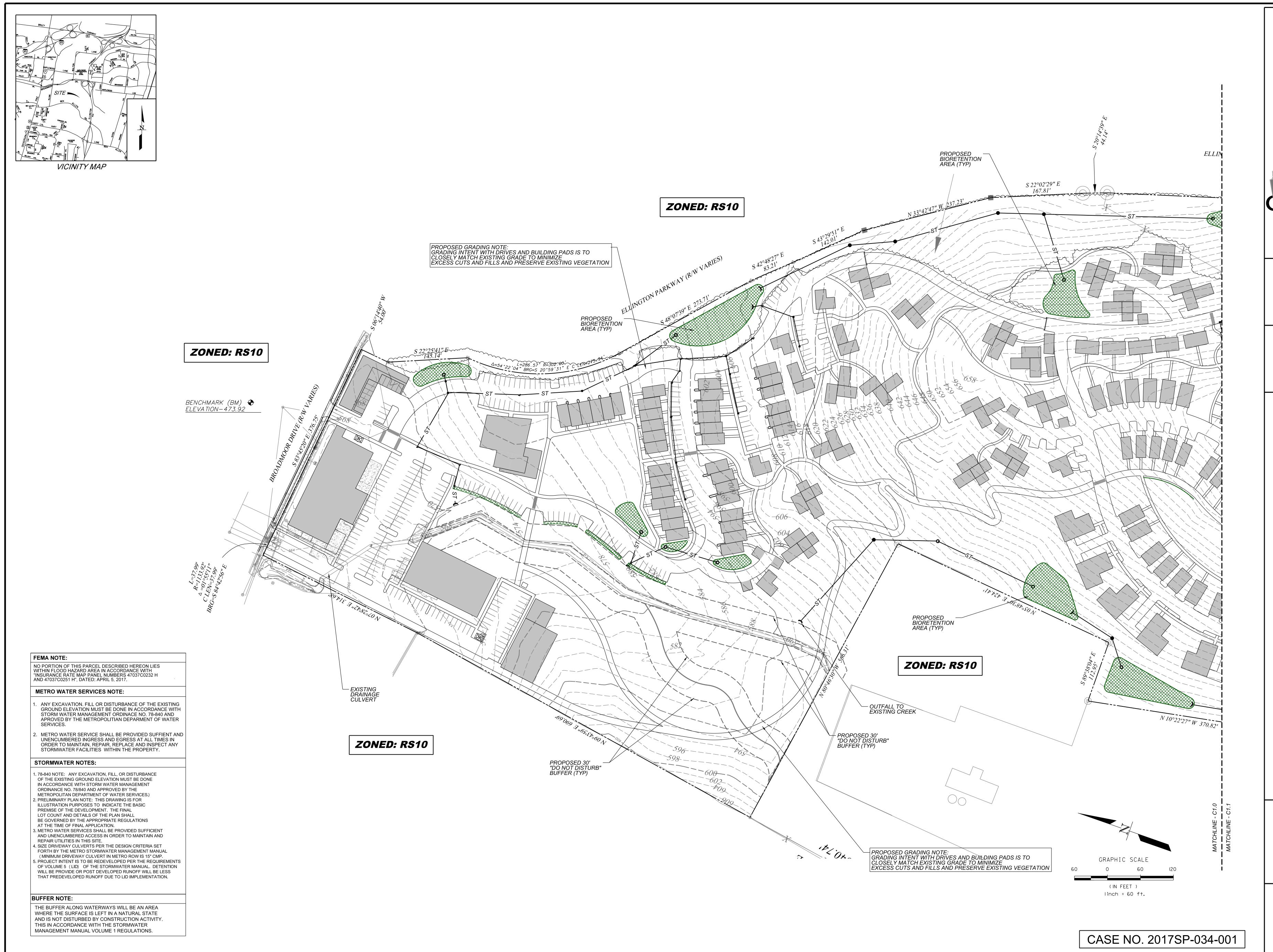
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SHEET:

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SITE UTILITY

CASE NO. 2017SP-034-001



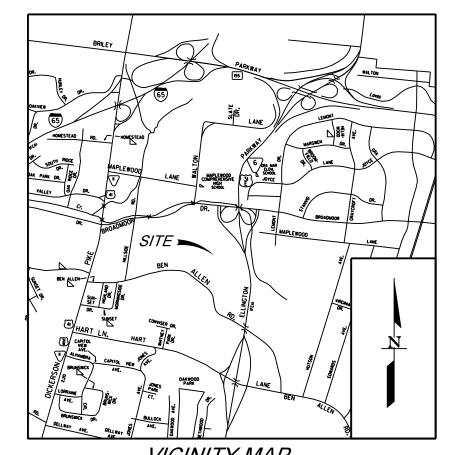
Barge Lauthen 6606 CHARLOTTE PIKE, STE 210 NASHVILLE, TENNESSEE 37209 6 \ 5.356.99 \ \ PHONE

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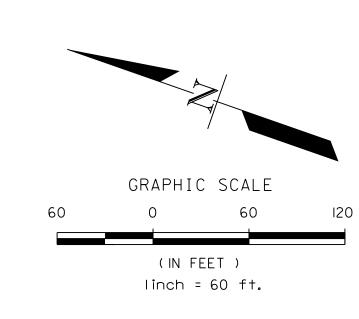
JOB NO.: 16-945 DATE: 3-30-17

SCALE: 1'' = 60'-0''SHEET:

> SITE GRADING AND DRAINAGE



VICINITY MAP



PROPOSED -DRAINAGE PROPOSED -BIORETENTION AREA (TYP)

PROPOSED GRADING NOTE:
GRADING INTENT WITH DRIVES AND BUILDING PADS IS TO
CLOSELY MATCH EXISTING GRADE TO MINIMIZE
EXCESS CUTS AND FILLS AND PRESERVE EXISTING VEGETATION

FEMA NOTE:

NO PORTION OF THIS PARCEL DESCRIBED HEREON LIES WITHIN FLOOD HAZARD AREA IN ACCORDANCE WITH "INSURANCE RATE MAP PANEL NUMBERS 47037C0232 H AND 47037C0251 H", DATED: APRIL 5, 2017.

METRO WATER SERVICES NOTE:

1. ANY EXCAVATION. FILL OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINACE NO. 78-840 AND APROVED BY THE METROPOLITIAN DEPARMENT OF WATER

. METRO WATER SERVICE SHALL BE PROVIDED SUFFIENT AND UNENCUMBERED INGRESS AND EGRESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE AND INSPECT ANY STORMWATER FACILITIES WITHIN THE PROPERTY.

STORMWATER NOTES:

1. 78-840 NOTE: ANY EXCAVATION, FILL, OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ORDINANCE NO. 78/840 AND APPROVED BY THE METROPOLITAN DEPARTMENT OF WATER SERVICES.) 2. PRELIMINARY PLAN NOTE: THIS DRAWING IS FOR ILLUSTRATION PURPOSES TO INDICATE THE BASIC PREMISE OF THE DEVELOPMENT. THE FINAL LOT COUNT AND DETAILS OF THE PLAN SHALL BE GOVERNED BY THE APPROPRIATE REGULATIONS AT THE TIME OF FINAL APPLICATION. 3. METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED ACCESS IN ORDER TO MAINTAIN AND REPAIR UTILITIES IN THIS SITE. 4. SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL (MINIMUM DRIVEWAY CULVERT IN METRO ROW IS 15" CMP. 5. PROJECT INTENT IS TO BE REDEVELOPED PER THE REQUIREMENTS OF VOLUME 5 (LID) OF THE STORMWATER MANUAL. DETENTION WILL BE PROVIDE OR POST DEVELOPED RUNOFF WILL BE LESS THAT PREDEVELOPED RUNOFF DUE TO LID IMPLEMENTATION.

BUFFER NOTE:

THE BUFFER ALONG WATERWAYS WILL BE AN AREA WHERE THE SURFACE IS LEFT IN A NATURAL STATE AND IS NOT DISTURBED BY CONSTRUCTION ACTIVITY. THIS IN ACCORDANCE WITH THE STORMWATER MANAGEMENT MANUAL VOLUME 1 REGULATIONS.



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615.352.6737 F A X

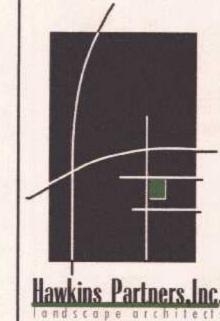
JOB NO.: 16-945 DATE: 3-30-17

SCALE: 1'' = 60'-0''SHEET:

SITE GRADING AND DRAINAGE

CASE NO. 2017SP-034-001





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EXP. 3/31/20

ALLEN MASTER PLAN

PRELIMINARY SP

DOADMOOR AND BEN ALL

JOB NO.: 16-945 DATE: 7-18-17

SHEET:

1.0

OVERVIEW