



MPC Case Number - 2017 SP-023-001

# BURKITT RIDGE

BURKITT ROAD AT WESTCOTT LANE

Preliminary Specific Plan

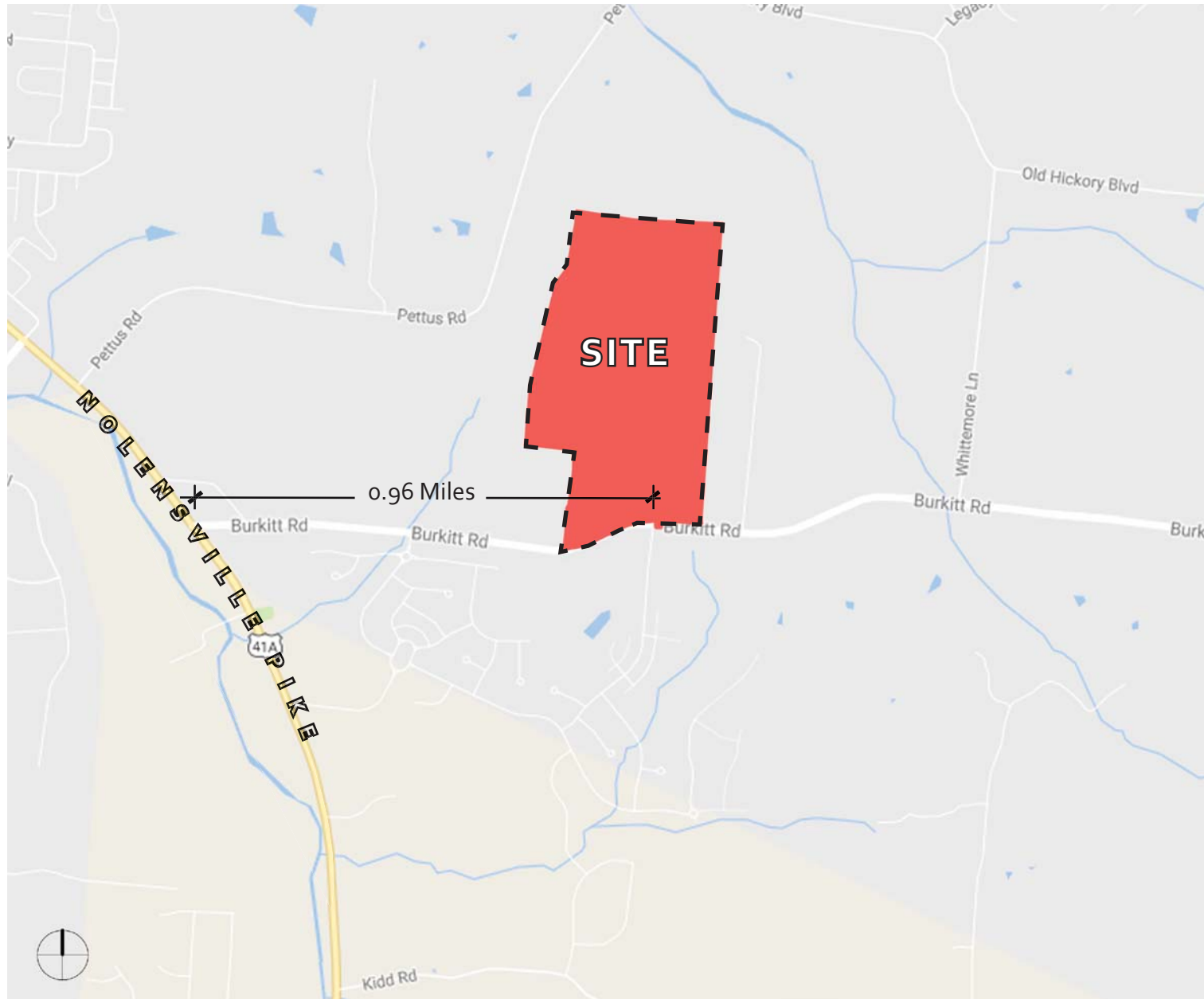
March 26, 2017

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# Site Vicinity Map



**Site Info**

Site is 143.92 acres of land located at:

0 Burkitt Road  
 Map & Parcel(s): 18700000100  
 Council District 31 - Fabian Bedne

**Owners of Record**

Stacy J. Carter  
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**Developer**

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# Existing Conditions

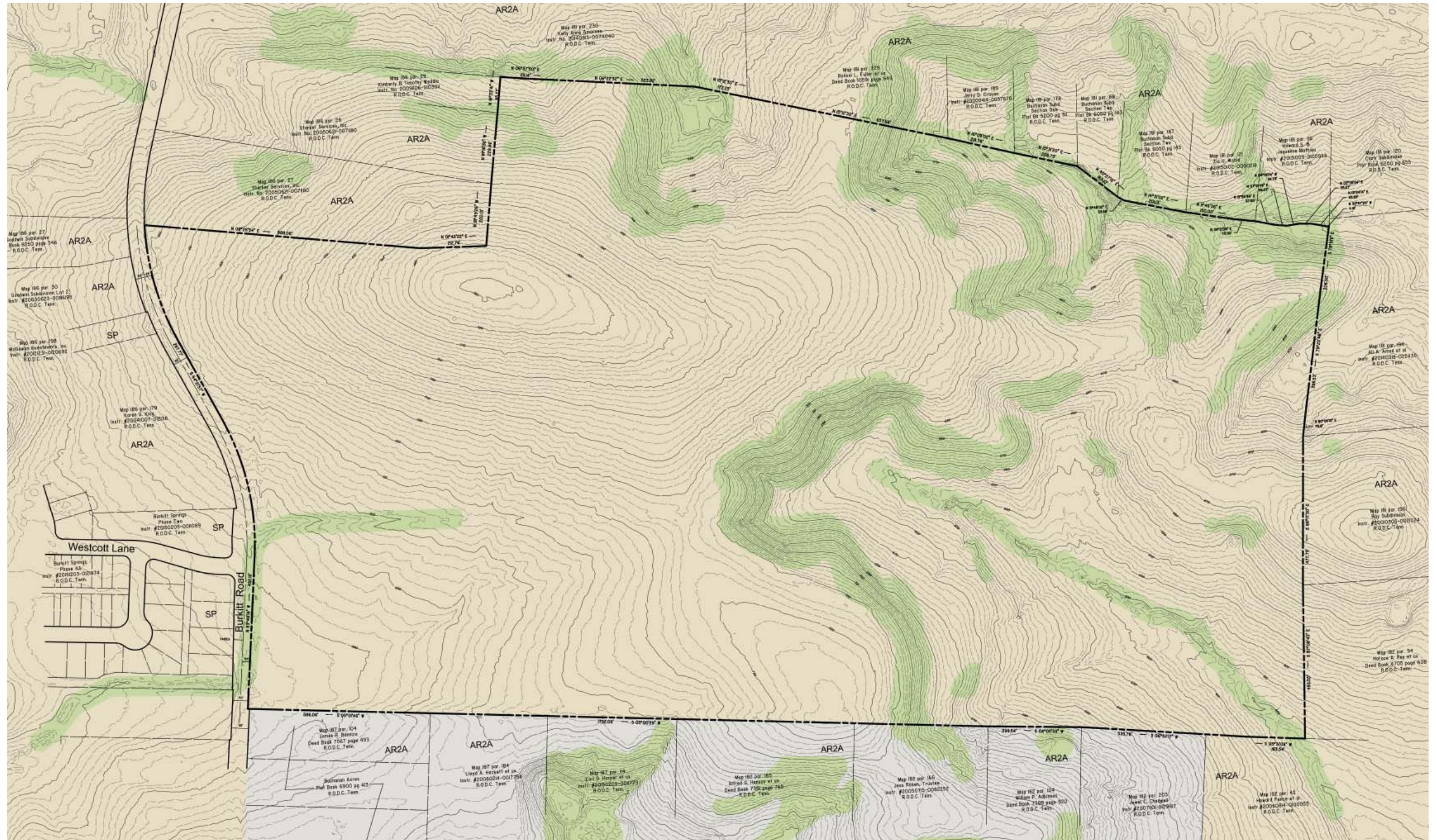
## Site Info

Site is 143.92 acres of land located at:

0 Burkitt Road  
 Map & Parcel(s): 18700000100  
 Council District 31 - Fabian Bedne

Current Zoning: AR2a

Land Use Policy: T3-NE Suburban  
 Neighborhood Evolving, Conservation





# Land Use Policy

The property is located within of the Southeast Community Plan, Davidson County. The current land use policy for the entire property is T3 Suburban Neighborhood Evolving and T3 Conservation. The SP site plan will retain the T3 NE and Conservation policies while introducing a policy amendment to include T3 Suburban Neighborhood Center. This policy amendment is necessary to integrate neighborhood serving retail which will be located at a prominent intersection off Burkitt Road.

## T3 Suburban Neighborhood Evolving - Current

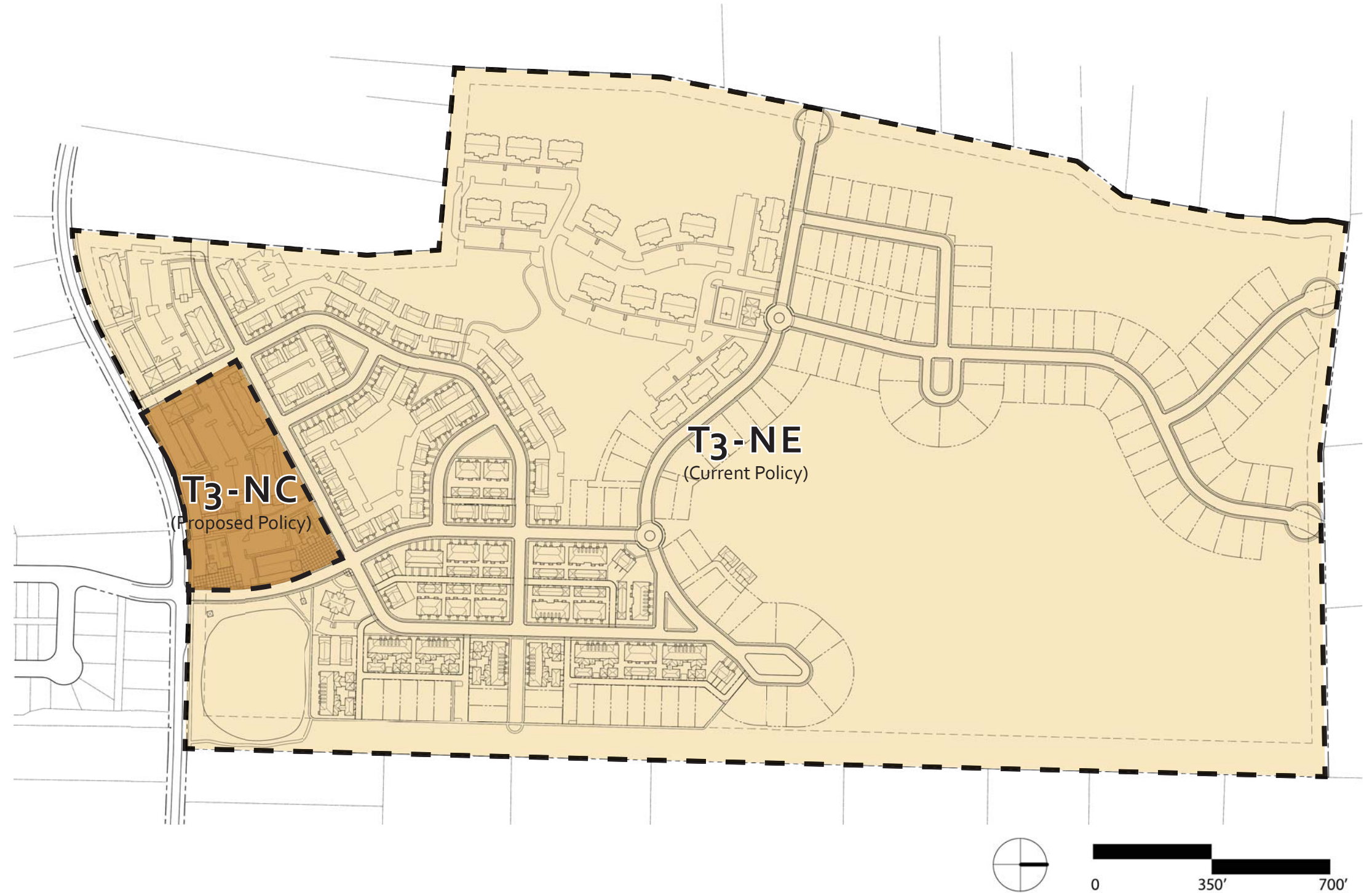
T3 Suburban Neighborhood Evolving areas policy intent is to promote a diverse housing mix with a high level of connectivity. T3 Suburban Neighborhood Evolving areas have moderate-density development patterns with residential and institutional land uses. The public realm and streetscape feature the consistent use of lighting and both formal and informal landscaping. T3 Suburban Neighborhood Evolving areas are served by moderate to high levels of connectivity with street networks, sidewalks, bikeways and mass transit leading to surrounding neighborhoods and open space. The edges of T3 Suburban Neighborhood Evolving areas are firm with clearly distinguishable boundaries identified by lot size, building placement, and environmental features.

## T3 Conservation - Current

Conservation Community Character policy is found in all Transect Categories except T6 Downtown. Its intent is to preserve environmentally sensitive land features through protection and remediation. Environmentally sensitive land features are kept in a natural state and any development is minimal to protect water quality, minimize infrastructure and public service costs, and preserve the unique environmental diversity. Natural areas that include jurisdictional streams, wetlands, and slopes greater than 20% shall not be disturbed.

## T3 Suburban Neighborhood Center - Proposed

T3 Suburban Neighborhood Centers serve suburban neighborhoods within a five minute drive, are generally located at intersections of suburban streets that contain mixed use, commercial, residential, and institutional land uses. Intensity is generally placed within edges of the T3 Suburban Neighborhood Center, not exceeding the four corners of an intersection with regularly spaced buildings built to the back edge of the sidewalk with parking provided on-street, beside or behind the buildings. The public realm and streetscape feature the consistent use of lighting and generally formal landscaping. T3 Suburban Neighborhood Centers are served by well-connected street networks, sidewalks and mass transit leading to surrounding neighborhoods and open space. The edges of T3 Suburban Neighborhood Centers are firm with distinguishable boundaries identified by land use, building types, building placement, and block structure.





## Design Plan

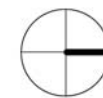
Through the Specific Plan, the community will be assured a level of quality and a sense of community. The intent of the Burkitt Ridge SP is to provide a comfortable, safe, mixed-use community with emphasis on pedestrian oriented streetscapes, diverse housing options, adequate open space and quality architecture.

More specifically, the SP is intended to:

- Assure the compatible integration of retail, office and institutional uses with residential uses.
- Assure the compatible integration of a variety of housing types: townhouses and multi-family and single family, in order to accommodate the housing needs of a diverse population.
- Maintain a scale and form of development that emphasizes sensitivity to the pedestrian environment.
- Minimize the impact of automobiles into the setting through strategies such as "shared parking," in which adjacent land uses having different peak-hour parking demands can share parking facilities.
- Minimize the need for vehicles to travel on Burkitt Road, or to travel significant distances on Burkitt Road, by providing neighborhood commercial uses within close proximity to residents.
- Achieve "traffic calming" benefits through: an integrated street network providing options for traffic flow, the avoidance of excessively wide streets, and the provision of on-street parking.
- Provide for a variety of strategically-located and carefully-designed common spaces, including streets, greens and natural areas.
- Encourage active ground floor uses, such as neighborhood retail, restaurants and office uses, to animate the street within the Neighborhood Center.



Front Yard, Typ.



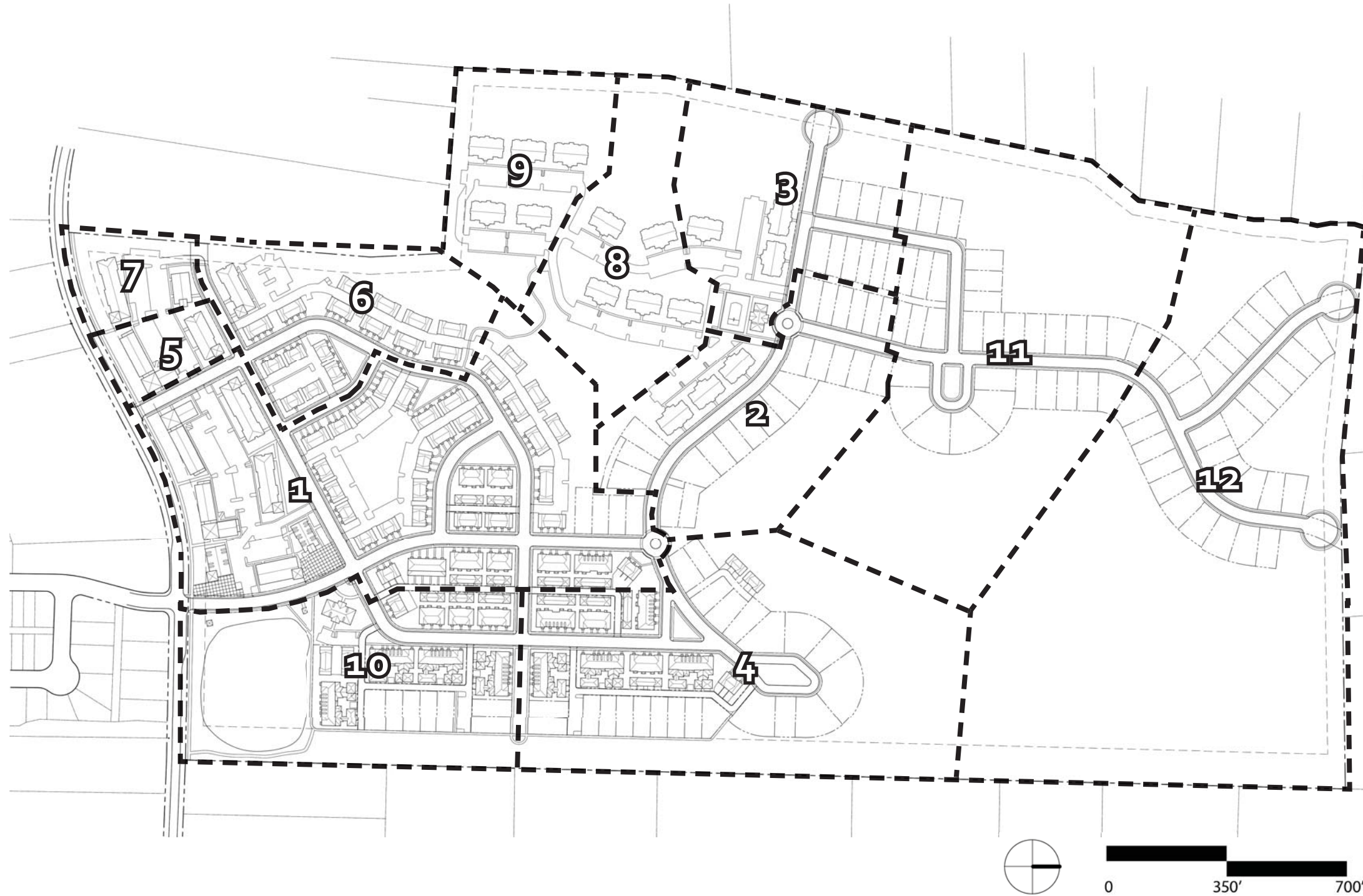


## Phasing Strategy

The Development Phasing Strategy boundaries and sequence of construction to final buildout shown on this plan is preliminary and subject to revisions based on market conditions and future final SP submittals. Appropriate infrastructure will be provided for each phase in coincident with the Phasing Plan as development continues internally.

## Development Schedule

It is anticipated that construction will begin in the 1st quarter of 2018 and will take approximately 8 years for full completion of all phases.



## Street Network Design






The Burkitt Ridge interconnected street network is designed to disperse automobile traffic while providing multiple entrances and access points for emergency vehicles (with two (2) connections). A total of six (6) potential access points from outside the site are proposed. Future street extensions are desired to further disperse traffic and to provide connections to the SP from future developments. All streets and alleys within the development are intended to be constructed within public easements for access and utilities.

Temporary Turn-Arounds shall meet ST331 - Residential Turn-Around Cul De Sac standard guidelines per Metro standards.



POTENTIAL FUTURE CONNECTION

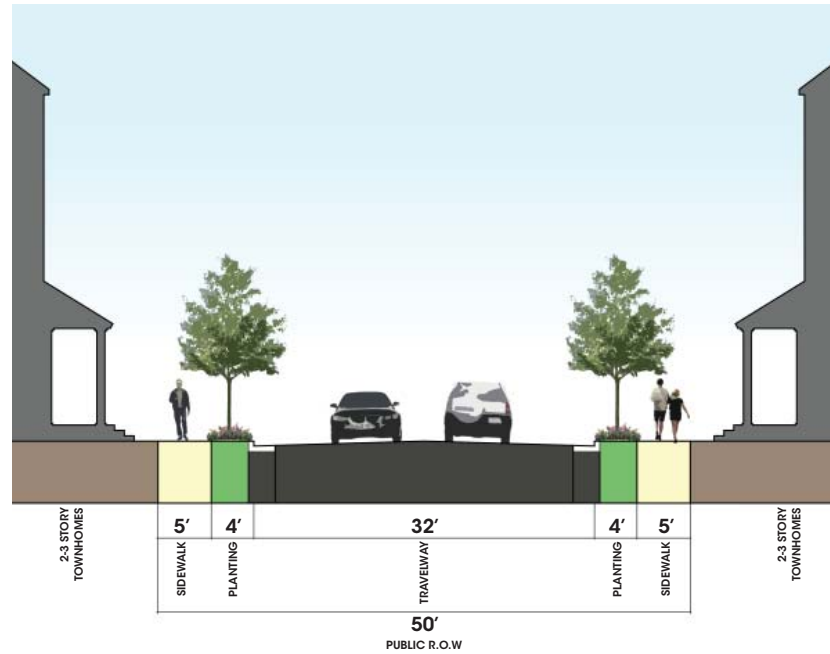
### Public Streets

-  Neighborhood Street- 50 feet Public R.O.W
-  Park Street- 46 feet Public R.O.W
-  Alley- 20 feet Public R.O.W
-  Vehicular Access
-  Future Street Connection

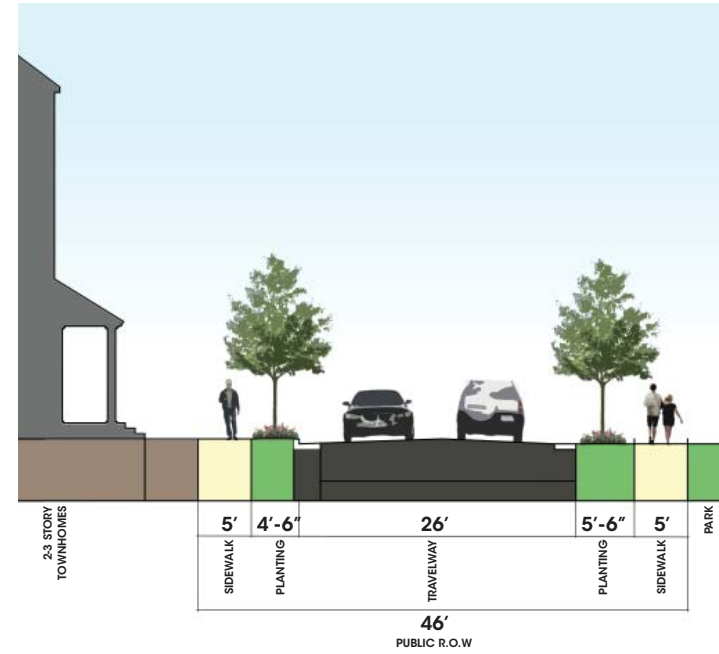


# Street Network

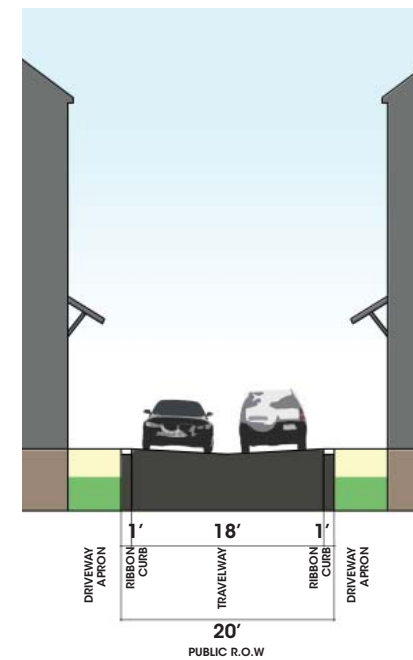
## Neighborhood Street



## Park Street



## Alley



## Street Sections

Each street type in Burkitt Commons is designed for specific conditions based on projected frequency of traffic, desired parking conditions, the types of uses and intensity of housing along that street, specific physical conditions and to allow for emergency vehicle access. While the Street Network/Street Sections illustrate the desired street layout and design, the plan shall be flexible to respond to physical site conditions, dispersion of building types, community desires and a changing market. The SP shall allow for variations in the design of the street network, street sections and block layout so long as it meets the intent of the regulations and are approved by Metro Public Works.

- All roadways shall be built with Metro Public Works standard ST-200 curb and gutters and Metro Public Works standard ST-210 sidewalks.
- Each street intersection shall be designed and engineered specifically to allow for a 39' x 9' fire engine/school bus to make turns free of parked cars and curbs. Final construction plans will provide turning diagrams assuring adequate turning movements.

## Burkitt Road

Project team will work with TDOT and Metro Nashville to appropriate R.O.W dedications for planned improvements of Burkitt Road.

## Standard Notes

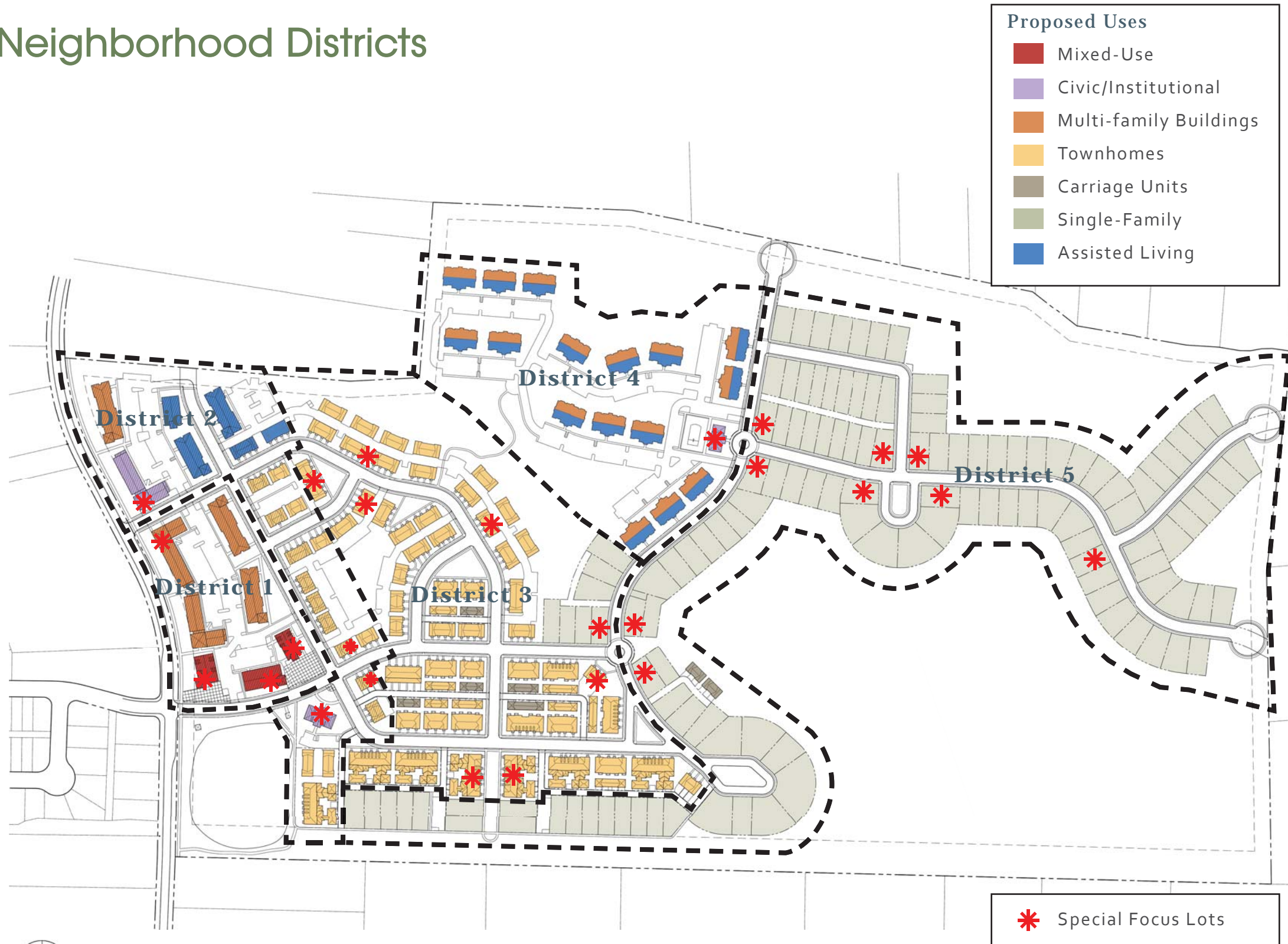
- Roadway Improvements that are a direct result of this specific project as determined by the approved Traffic Impact Study and the Department of Public Works shall be constructed. Final plans and road grades shall comply with the design regulations established by the Department of Public Works.
- Any required right-of-way within the project site that is identified as necessary to meet the adopted roadway plans shall be dedicated.
- In compliance with the Major and Collector Street Plan requirements, a 6 foot planting strip and 8 foot sidewalk shall be provided along Burkitt Road. Applicant shall coordinate with Public Works and TDOT to assure the location is consistent with future plans for Burkitt Road.
- The final site plan/ building permit site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.

## Alleys

- Rear loaded units shall provide access from alleys. The intent to limit the amount of direct access from the street network allowing better traffic flow across the system.
- Alleys are intended to circumnavigate throughout the site to make accessing units easier from each street and to conceal parking and utilities from public view.



# Neighborhood Districts



## Districts

Burkitt Ridge is designed as a mixed use community, integrating diverse housing types, retail/restaurant, office and community services.

### District 1

The Neighborhood Center is intended to accommodate denser residential, civic, office and neighborhood retail uses. Retail, live/work and office uses will be concentrated on the ground level of several mixed use buildings with office and/or residential uses on the upper floors.

### District 2

Denser multifamily housing, townhomes, assisted living and civic uses shall be limited to around the Neighborhood Center and along Burkitt Road, transitioning appropriately in scale and density to the remainder of the community.

### District 3

The heart of the residential community, District 3 contains primarily townhomes of moderate density. Single family development is provided along the perimeter serving as a transition to the adjoining property and single family district.

### District 4

This pocket within the Ridge is envisioned as an inclusive multifamily community with the possibility of some assisted living units with access to a central clubhouse and a trail to the neighborhood center.

### District 5

Lower density alley and traditional single family homes will be provided within this district with the opportunity to provide carriage units in limited instances along rear alley ways where provided.

*The future location of proposed uses in all districts may be modified based on changing market conditions so long as the intent and the principles of the SP are met.*

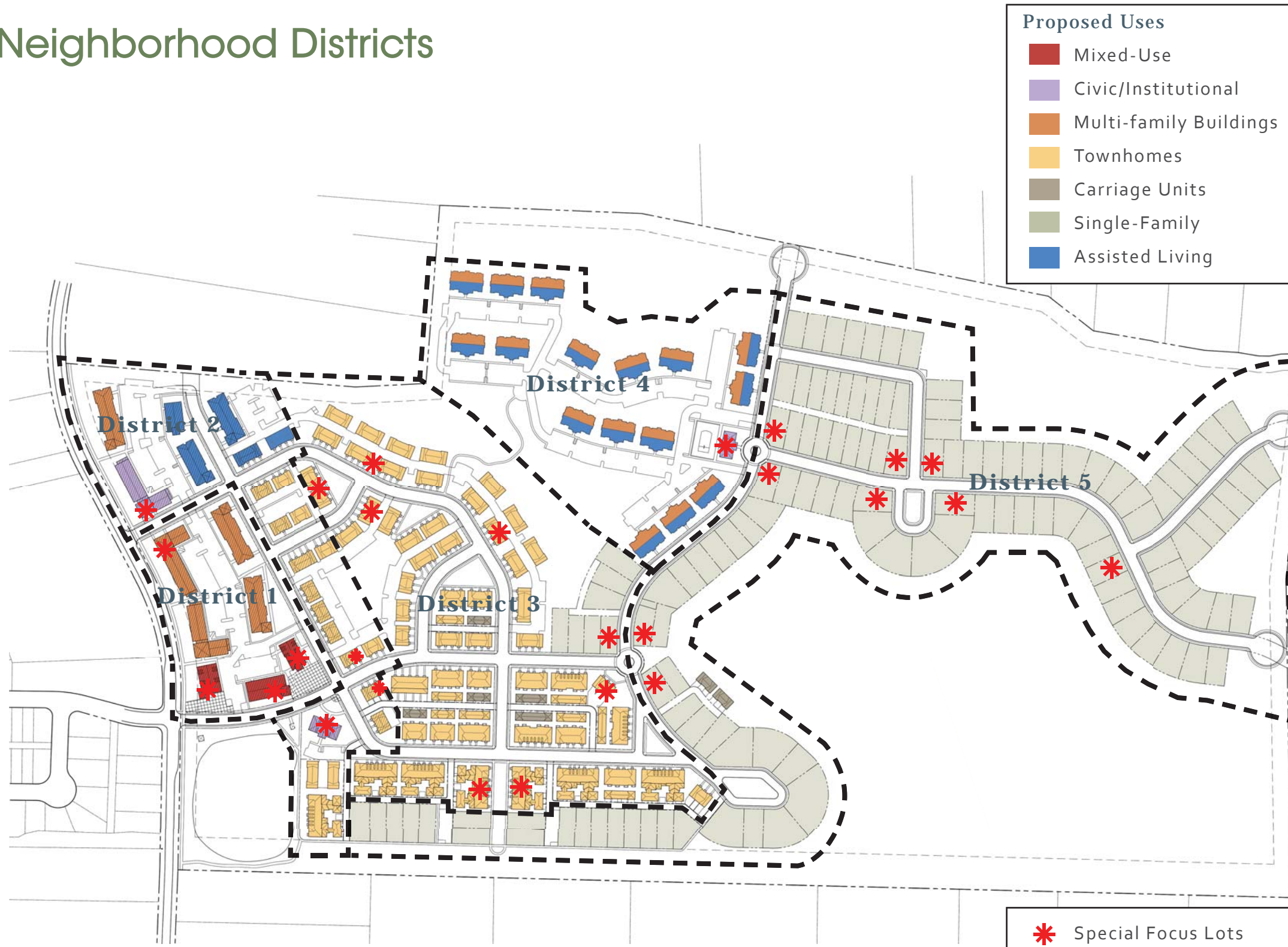
*See following pages for regulations and design guidelines associated with each building typology. Bulk standards provided shall supersede the corresponding base zoning bulk standards.*

### Special Focus Lots:

- Where possible, streets shall terminate on axis with a primary building form or architectural feature or on open space at the tee intersections.
- On corner lots, architectural features must address both the front and side streets (e.g. corner porches, side porches, bay windows, etc.) and blank side walls shall be avoided.



# Neighborhood Districts



## Districts

	DISTRICT 1	DISTRICT 2	DISTRICT 3	DISTRICT 4	DISTRICT 5
Acreage	5.88	11.64	23.10	14.82	39.52
Maximum Residential Units <sup>1</sup>	117	208	215	224	160
Maximum Assisted Living	0	100	0	84	0
Maximum Live-work units	30	0	0	0	0
Maximum Live-work Commercial (GSF)	16,500	0	0	0	0
Maximum Commercial Space (GSF)	17,000	0	0	0	0
Maximum Institutional Space (GSF)	0	per METRO	0	0	0
Density	19.8 du/ac	17.9 du/ac	9.3 du/ac	15.1 du/ac	4 du/ac

1- includes assisted living and live/work residential units as an aggregate

OVERALL TOTALS	
Total Site Acreage	143.92 acres
District Net Acreage	94.96 acres
<b>Maximum Residential Units<sup>1</sup></b>	<b>800 units</b>
Maximum Assisted Living Units	184 units
Maximum Live/Work Units	30 units
Maximum Live-work Commercial (GSF)	16,500 GSF
Maximum Commercial Space (GSF)	17,000 GSF
Gross Density	5.55 du / acre
District Net Density	8.42 du / acre

1 - includes assisted living and live/work units as aggregate







## Mixed-Use Buildings

**Intent:** Mixed-Use Buildings are intended to serve the neighborhood with retail, restaurant, office and residential uses. Ground floors should have active storefronts along street facades.

**Front Build-to Line:** Where buildings directly front public streets or common areas (e.g. sidewalks, parking, open space), 70% of the front facade must be built to the back of the proposed sidewalk.

**Minimum Building Separation:** 6 feet.

**Rear Setback:** 0 feet.

**Side Setback:** 0 feet.

**Maximum Height:** 3 1/2 stories in 50 feet. Building Height shall be measured from the average elevation of the natural grade to the top of the roof ridge.

- An additional "basement" level may be provided internally to the development where topography allows for additional sub-surface development including parking and conditioned space.

**Minimum Ground Floor Height:** 12 feet. Height shall be measured from the FFE to the ceiling.

**Primary entrance / Glazing :** Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 40% glazing (calculated as a % of ground floor facade measured from finished floors to ceiling).

**Off Street Parking Spaces:** Per Metro Zoning Code.

**Off Street Parking Location:** Where provided, off street parking shall be located behind and/or between buildings with the exception of single loaded access drives.

Single loaded access drives shall be limited to one parking bay and aisle perpendicular to the street (42 feet max pavement width); parking shall be set back a min. of 20 feet from street ROW.

**Passages:** Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

SEE REGULATION SHEET FOR ADDITIONAL DESIGN STANDARDS





## Civic/Institutional Buildings

**Intent:** Civic/Institutional Buildings are intended to serve the neighborhood with noncommercial and community serving uses.

**Front Setback:** Front setbacks for civil buildings will vary. Some buildings may be built to the back of the sidewalk for a more urban experience while others may be setback substantially within the site in more prominent position.

**Minimum Building Separation:** 10 feet.

**Rear Setback:** 0 feet.

**Side Setback:** 0 feet.

**Maximum Height:** 3 1/2 stories in 50 feet. Building Height shall be measured from the average elevation of the natural grade to the top of the roof ridge.

- An additional "basement" level may be provided internally to the development where topography allows for additional sub-surface development including parking and conditioned space.

**Minimum Ground Floor Height:** 12 feet. Height shall be measured from the FFE to the ceiling.

**Primary entrance / Glazing :** Building facades fronting a street are encouraged to provide a minimum of one principal entrance (doorway) and a minimum of 40% glazing (calculated as a % of ground floor facade measured from finished floors to ceiling). Reductions may be appropriate for some buildings based on the nature of the use (gov't buildings, gymnasiums, labs, etc).

**Off Street Parking Spaces:** Per Metro Zoning Code.

**Off Street Parking Location:** Where provided, off street parking shall be located behind and/or between buildings with the exception of single loaded access drives.

Single loaded access drives shall be limited to one parking bay and aisle perpendicular to the street (42 feet max pavement width); parking shall be set back a min. of 20 feet from street ROW.

**Passages:** Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

SEE REGULATION SHEET FOR ADDITIONAL DESIGN STANDARDS





## Multi-family Buildings

**Intent:** Multi-Family buildings are intended to provide an opportunity for diverse living options. Regulations and guidelines for Multi-Family buildings include amenity and accessory structures.

**Front Build-to Line:** Where buildings directly front public streets or common areas (e.g. sidewalks, parking, open space), the front facade must be built to within 0-15 feet from the back of the proposed sidewalk.

**Minimum Building Separation:** 10 feet.

**Rear Setback:** 0 feet.

**Side Setback:** 0 feet.

**Minimum Alley Setback:** 5 feet or greater than 15 feet from edge of pavement.

**Maximum Height:** 3 1/2 stories in 50 feet. Building Height shall be measured from the average elevation of the natural grade to the top of the roof ridge.

- An additional "basement" level may be provided internally to the development where topography allows for additional sub-surface development including parking and conditioned residential.

**Primary entrance / Glazing :** Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing (calculated as a % of ground floor facade measured from finished floors to ceiling).

**Foundation Height:** A raised foundation of 16"- 36" is required for all residential structures along the street frontage. Modifications may be granted by Planning Staff with the Final SP application for topographically challenged areas, ADA required units or unique circumstances.

**Off Street Parking Spaces:** 1 per Bedroom; 2 per Unit Max.

**Off Street Parking Location:** Where provided, off street parking shall be located behind and/or between buildings with the exception of single loaded access drives.

Single loaded access drives shall be limited to one parking bay and aisle perpendicular to the street (42 feet max pavement width); parking shall be set back a min. of 20 feet from street ROW.

**Passages:** Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

SEE REGULATION SHEET FOR ADDITIONAL DESIGN STANDARDS







## Townhomes

**Front Build-to Line:** Where buildings directly front public streets or common areas (e.g. sidewalks, parking, open space), the front facade must be built between 5-15 feet from the back of the proposed sidewalk.

**Minimum Building Separation:** 10 feet.

**Side Setback:** 0 feet

**Minimum Alley Setback:** 5 feet or greater than 15 feet from edge of pavement.

**Maximum Height:** 3 1/2 stories in 50 feet. Building Height shall be measured from the average elevation of the natural grade to the top of the roof ridge.

- An additional "basement" level may be provided internally to the development where topography allows for additional sub-surface development including parking and conditioned residential.

**Foundation Height:** A raised foundation of 16"- 36" is required for all residential structures along the street frontage. Modifications may be granted by Planning Staff with the Final SP application for topographically challenged areas, ADA required units or unique circumstances.

**Primary entrance / Glazing :** Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing (calculated as a % of ground floor facade measured from finished floors to ceiling).

**Off Street Parking Spaces:** 1 per Bedroom; 2 per Unit Max.

**Off Street Parking Location:** Where provided, off street parking shall be located behind and/or between buildings with the exception of single loaded access drives.

Single loaded access drives shall be limited to one parking bay and aisle perpendicular to the street (42 feet max pavement width); parking shall be set back a min. of 20 feet from street ROW.

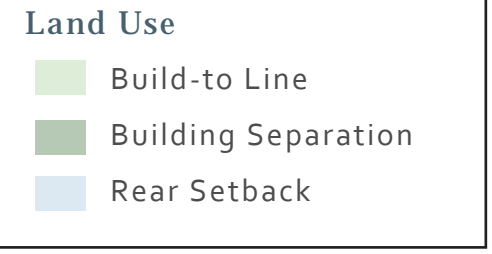
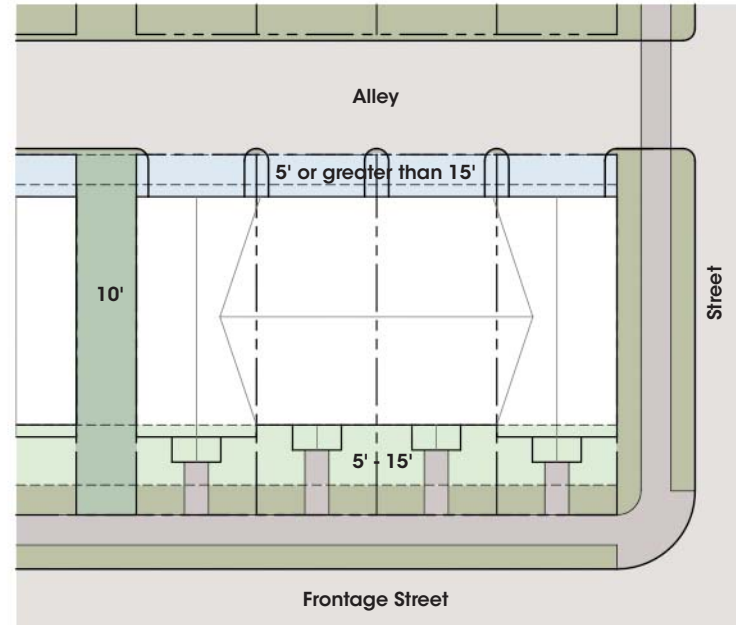
**Passages:** Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

SEE REGULATION SHEET FOR ADDITIONAL DESIGN STANDARDS



# Bulk Standards

## City Townhome



## Townhomes

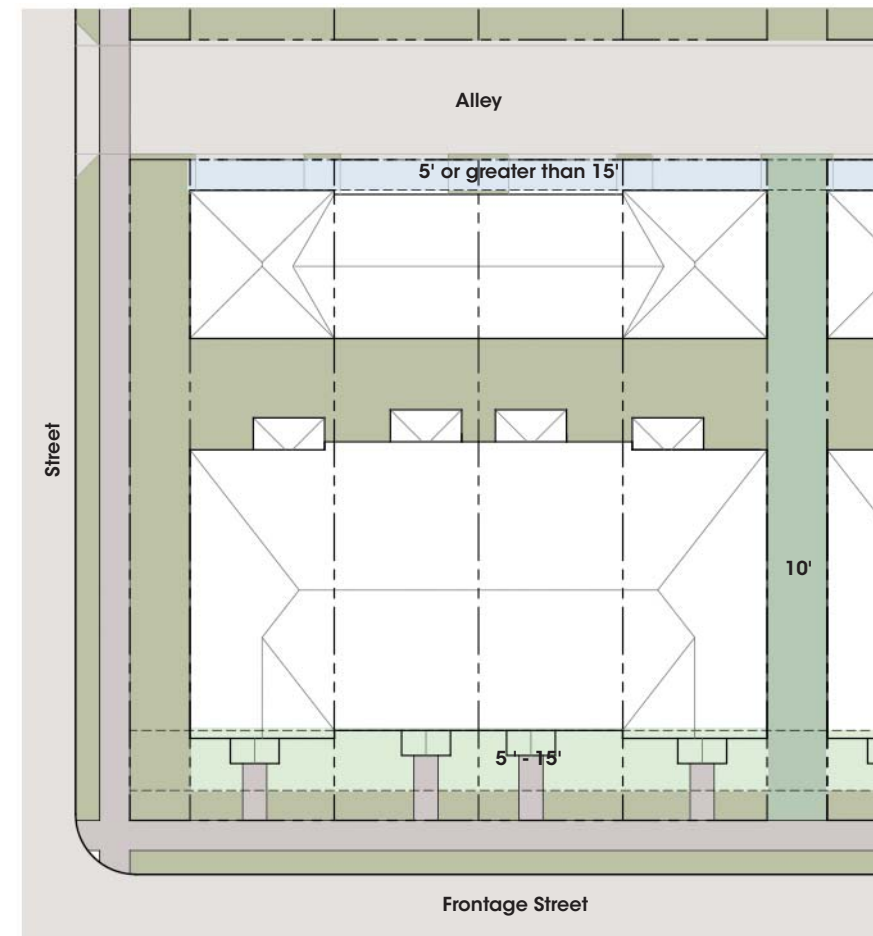
The following townhome typologies are provided throughout Burkitt Ridge to offer a variety of housing options.

**City Townhome:** A park-under townhome accessed from a rear alley. A flex space on the ground floor faces the frontage street.

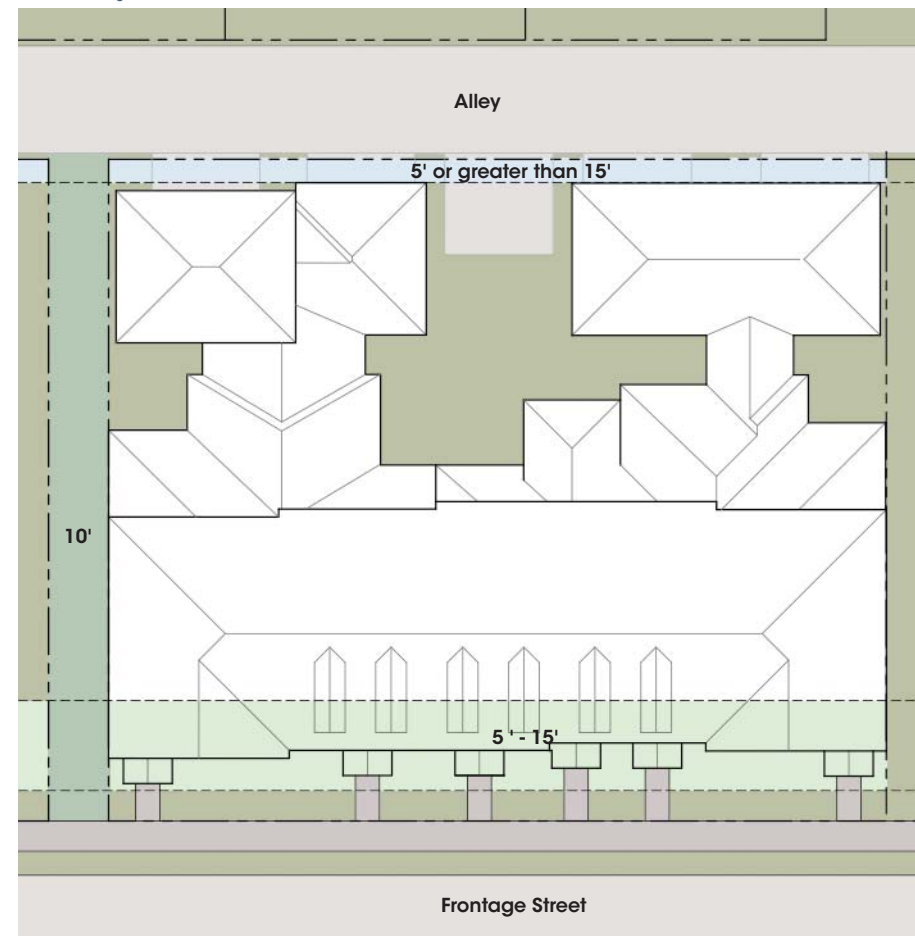
**Garden Townhome:** A townhome with a common detached garage accessed from a rear alley. A flex space on the ground floor faces the frontage street.

**Courtyard Townhome:** A townhome with a shared detached garage framing private courtyards. Vehicular access is from a rear alley. A flex space on the ground floor faces the frontage street.

## Garden Townhome



## Courtyard Townhome







## Carriage Units

**Minimum Width:** 16 feet.

**Minimum Depth:** 10 feet.

**Front Build-to Line:** None.

**Minimum Building Separation:** 6 feet (when detached).

**Rear Setback:** 0 feet.

**Side Setback:** 0 feet

**Minimum Alley Setback:** 5 feet or greater than 15 feet from edge of pavement.

**Maximum Height:** 3 1/2 stories in 45 feet. Building Height shall be measured from the average elevation of the natural grade to the top of the roof ridge.

**Off Street Parking Spaces:** 1 per Bedroom; 2 per Unit Max.

**Off Street Parking Location:** Off street parking and garage access shall be from an alley.

- Carriage units shall be limited to internal parking lot areas or adjacent to an alley or passive open space; carriage units shall not have direct frontage on streets or formal open space.

SEE REGULATION SHEET FOR ADDITIONAL DESIGN STANDARDS





## Single-family Detached

### Front Loaded:

- **Minimum Lot Area:** 4,600 sq. ft.
- **Minimum Lot Width at Front Setback:** 46 feet.
- **Minimum Lot Depth:** 100 feet.
- **Minimum Side Yard Setback:** 5 feet.
- **Rear Setback:** 20 feet.

• Front loaded garages shall be set back 10 feet behind closest portion of structure to the ROW whether it be conditioned space or a porch, which measures a min. of 80% of the width of the front facade of the unit (excluding garage).

• Front entry garages on corner lots shall be accessed from the secondary street.

### Rear Loaded:

- **Minimum Lot Area:** 3,500 sq. ft.
- **Minimum Lot Width at Front Setback:** 35 feet.
- **Minimum Lot Depth:** 100 feet.
- **Minimum Side Yard Setback:** 3 feet.
- **Minimum Alley Setback:** 5 feet or greater than 15 feet from edge of pavement.

**Front Build-to Line:** 10-25 feet from front property line.

**Off Street Parking Location:** Off street parking and garage access shall be from an alley.

**Maximum Height:** 3 1/2 stories within 45 feet. Building Height shall be measured from the average elevation of the natural grade to the top of the roof ridge.

• An additional "basement" level may be provided internally to the development where topography allows for additional sub-surface development including parking and conditioned residential.

**Foundation Height:** A raised foundation of 16"- 36" is required for all residential structures along the street frontage. Modifications may be granted by Planning Staff with the Final SP application for topographically challenged areas, ADA required units or unique circumstances.

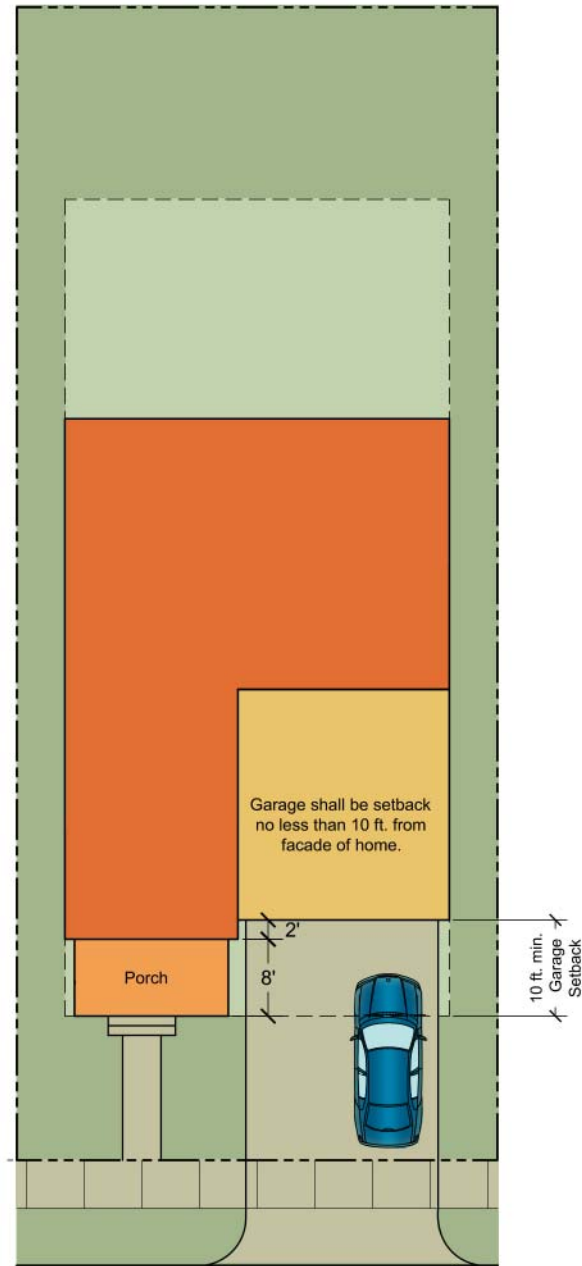
**Off Street Parking Spaces:** 2 per Unit Min.

**Passages:** Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

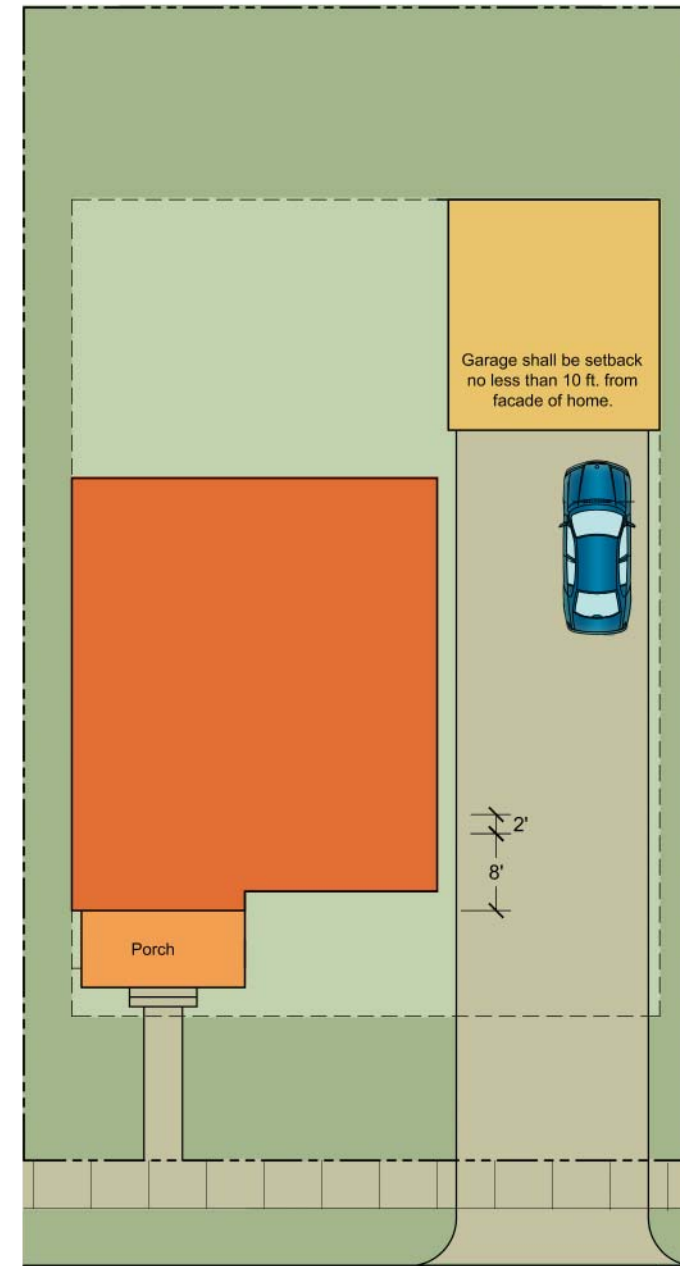
SEE REGULATION SHEET FOR ADDITIONAL DESIGN STANDARDS



Attached Garage



Detached Garage



## Single-family Detached

**Garage Recess and Placement:** Recessed garage is defined as being a minimum of 10 feet from front building facade.

- Front entry garages on corner lots shall be placed opposite the street corner.
- Side entry garages on corner lots shall be placed towards the rear of the lot and accessed from the secondary street.
- Lots served by an alley shall have rear entry garages.
- Rear entry garages shall either be sited 5 feet from rear property line or greater than 20 feet from the rear property line.

**Garages:** Garages shall be designed as secondary structures to the main house and shall be of consistent form, style, materials, and quality as the main house. Garages should be sited to minimize their visibility from, and impact on, the street.

Double bay garage doors shall be articulated to appear as two distinct single-bay doors. Articulation may include the variation of trim profiles and depth, window style and arrangement, door hardware and color scheme.

Where required, front entry garage doors shall be recessed beyond the front facade a minimum of 10 feet.

Garage doors are encouraged to be finished with dark colors to minimize their appearance and highlight the homes primary architectural features. Lighter color options are subject to review by METRO Planning.

**Front-Loaded Units:** Approximately 127 Single Family Lots are proposed for Burkitt Ridge.





## Detached Garages/Accessory Structure

**Front Build-to Line:** None.

**Minimum Side Yard Setback:** 3 feet.

**Minimum Rear Yard Setback:** 3 feet.

**Maximum Height:** 2 stories within 25 feet. Building Height shall be measured from the average elevation of the natural grade to the top of the roof ridge.

- Detached garages shall be limited to internal parking lot areas or as an accessory use located on an alley.

SEE REGULATION SHEET FOR ADDITIONAL DESIGN STANDARDS



## Landscape Standards

The developer of this project shall comply with the requirements of the SP and adopted tree ordinance 2008-328 (Metro Code Chapter 17.24).

- Street trees shall be provided along all street frontages at a minimum spacing average of fifty (50) linear feet.
- All common area landscaping shall be properly irrigated and maintained by a landscape contractor; if drought resistant plant material is used, irrigation shall not be required.
- Where trees are planted in rows, they shall be uniform in size and shape. All plants shall be freshly dug, sound, healthy, vigorous, well branched, free of disease, insect eggs, and larvae, and shall have adequate root systems.
- All container grown material shall be healthy, vigorous, well-rooted plants and established in the container in which they are sold. The plants shall have tops which are good quality and are in a healthy growing condition. All root bound plants shall be rejected.
- Groups of shrubs shall be in a continuous mulch bed with smooth continuous lines. All mulched bed edges shall be curvilinear in shape following the contour of the plant mass. Trees located within four feet of shrub beds shall share same mulch bed.
- Plant locations may be adjusted in the field as necessary to be clear of drainage swales and utilities. Finished planting beds shall be graded so as to not impede drainage away from buildings. If significant relocations are required, contractor shall contact landscape architect for resolution. Failure to make such relocations known to the owner or landscape architect will result in contractor's liability of plant materials.
- Trees must remain vertical and upright for the duration of the guarantee period. Guys and strapping shall be removed after one growing season.
- The root crown to be at finished grade or no greater than a maximum of one inch higher (after settling) than finished grade.
- A 100 foot buffer zone shall be provided on the Eastern property boundary to separate and buffer the development from the Gloryland Lane adjacent properties. The protection of trees shall follow the requirements of the zoning code 17.24.110. A construction fence shall be placed along this buffer prior to any site grading. Impacted trees will be replaced per the zoning requirements for the replacement of trees 17.24.100.





# Regulations

## Zoning

If a development standard, not including permitted uses, is absent from the SP plan and/or Council approval, the property shall be subject to the standards, regulations and requirements of the MUL-A zoning district as of the date of the applicable request or application. Uses are limited as described in the Council ordinance.

Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.

## Permitted Uses

All uses permitted by the Mixed-Use Limited Alternative (MUL-A) designation.

<b>Commercial (Limited to District 1):</b>	<b>17,000 SF Max.</b>
<b>Live / Work (Limited to District 1):</b>	<b>16,500 Max.</b>
<b>Institutional / Civic:</b>	<b>TBD by METRO Government</b>
<hr/>	
<b>Total Residential Aggregate:</b>	<b>800 Units Max.</b>
• General Residential:	800 Units Max.
• Live/Work:	30 Units Max.
• Ind./Assisted Living:	184 Units Max.

- Accessory uses associated with residential (Live/Work, Clubhouse, Fitness Center, Leasing Office, etc.) shall not be considered a commercial or institutional use for the purposes of calculating maximum square footage.

## Home Occupancy

Home occupation shall be allowed as an accessory use in all districts as established per Section 17.16.250D of the METRO Zoning Ordinance.

## METRO Fire Station

Applicant will work with METRO General Services and METRO Fire to coordinate the best location, design and access for a Fire Station. The Fire Station will be located within Districts 1 or 2 that provide access to Burkitt Road.

## Live / Work Regulations

Live/works units shall be considered an accessory use to a townhome residence and shall comply with all bulk requirements herein for townhome development. All live/work units shall meet local building code standards.

**Commercial** - Live/work units shall be considered an accessory retail or office use on the ground floor of a residence subject to the following:

1. Limited to District 1 of the site.
2. The commercial use shall be conducted in a dwelling unit by one or more occupants of the dwelling unit. No more than one part-time or full-time employee not living within the dwelling may work at the location.
3. The retail or office use shall not occupy more than five hundred fifty square feet of ground floor area.
4. The use of mechanical or electrical equipment shall be permitted in connection with a commercial use provided such equipment:
  - a. Complies with the Metro noise ordinance
  - b. Does not interfere with radio and television reception on neighboring properties.
5. The storage of materials or goods shall be permitted in connection with a commercial use provided such storage complies with the following standards.
  - a. Only those materials or goods that are utilized or produced in connection with the commercial use may be stored within the dwelling unit and must be stored completely within the space designated for commercial activities.
  - b. All flammable or combustible compounds, products or materials shall be maintained and utilized in compliance with Fire Code NFPA-30.
6. Offensive noise, vibration, smoke, dust or other particulate matter, odorous matter, heat, humidity, glare, or other objectionable effects shall not be permitted.
7. The manufacture or repair of transportation equipment shall not be permitted.
8. Commercial uses shall provide parking based on the intended use in accordance with Metro parking requirements 17.20.
9. Any signage associated with the commercial use shall be limited to the ground floor. A wall, shingle or projecting sign measuring no greater than 100 sf shall be permitted per building. Internal illumination of signage is prohibited.

## Parking

Unless otherwise specified, provided parking shall meet the requirements of the parking standards of the Zoning Code (17.20).

Parking shall be provided within parking areas and private garages internal to the development. Guest parking spaces shall be provided in the form of on-street parking throughout the SP.

Bicycle Parking shall be provided per Metro requirements.

See "Live/Work regulations" for parking requirements associated with live/work units.

## Architectural Standards

Buildings shall avoid continuous uninterrupted blank facades. At a minimum, the facade plane shall be interrupted by one of the following for every thirty (30) linear feet of street frontage:

- A change in building material
- A horizontal undulation in the building facade of two (2) feet or greater
- A porch, stoop, bay window or balcony; porches shall be a min. six (6) feet in depth
- Common pedestrian passages shall be provided between parcels to allow exterior access from front to back of buildings at reasonable intervals.
- Refuse collection, recycling and mechanical equipment shall be fully screened from public view by the combination of fences, walls or landscaping.
- Windows shall be vertically oriented at a ratio of 1.5:1 or greater; Planning staff may allow modifications to this standard for dormers, decorative windows, clerestory windows, egress windows and other special conditions.
- EIFS, vinyl siding and untreated wood shall be prohibited for all uses (vinyl soffits shall be permitted).
- HVAC units shall be located at the rear half of the side of unit, behind the unit, or on the roof of each building.





# Regulations

## Signage

The following sign types shall be permitted: Projecting Signs, Wall Mounted Signs, Awning Signs, Ground Mounted Signs, Canopy Signs.

- Ground mounted signs shall be limited to twelve (12) feet in width and six (6) feet vertically of signage area.
- Signs may additionally include a maximum of a two (2) feet masonry foundation.
- External Illumination: Light sources shall be directed towards sign minimizing glare to the sidewalk or adjacent properties.
- Internal Illumination: Signs are permitted to be internally illuminated but the sign background must be opaque with only graphic, text or logos being illuminated.
- Signs standards not addressed herein must meet the requirements permitted by the Mixed-Use Limited Alternative (MUL-A) base zoning designation.

## Standard Notes

- The purpose of this SP is to receive preliminary approval to permit the development of a one hundred forty three (143)+ acre mixed-use development as shown.
- The final site plan/ building permit site plan shall depict the required public sidewalks, any required grass strip or frontage zone and the location of all existing and proposed vertical obstructions within the required sidewalk and grass strip or frontage zone. Prior to the issuance of use and occupancy permits, existing vertical obstructions shall be relocated outside of the required sidewalk. Vertical obstructions are only permitted within the required grass strip or frontage zone.
- Right-of-way required within the project site that is identified as necessary to meet the adopted roadway plans shall be dedicated.

## Development Standards

- Overall FAR = 0.40 Max.
- Overall ISR = 0.60 Max.



# Grading Layout

## Legend

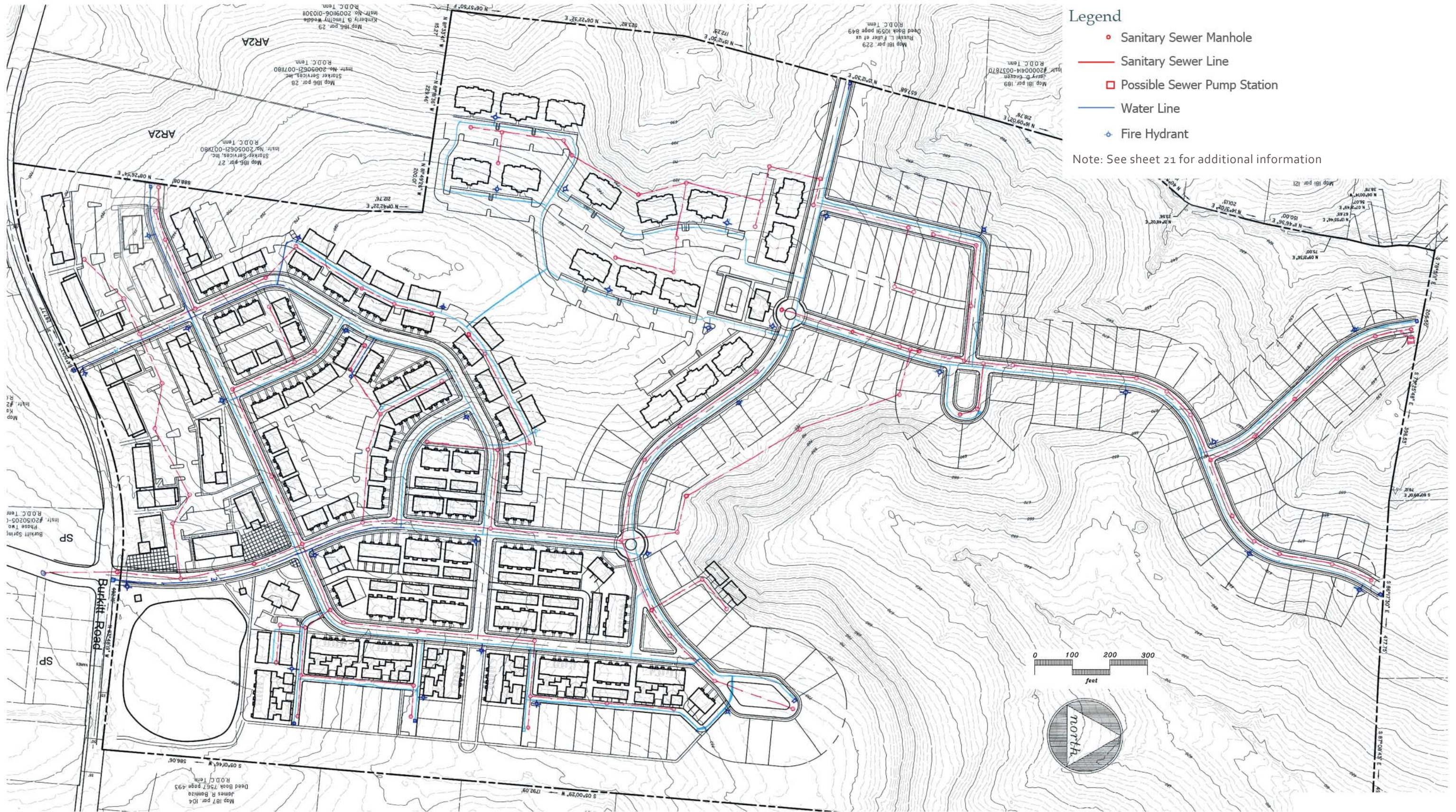
- Proposed Grading (2' Contours)
- Possible Water Quality and Detention Area
- Buffer
- Possible Waters of the State

Note: See sheet 21 for additional information





# Water and Sewer Layout





## Grading and Drainage General Notes

- No grading, stripping, filling, or other disturbance of natural ground cover shall take place prior to the approval of an erosion control plan.
- Existing 2' contours shown taken from USGS digital elevation data.
- Property located in zone "X" on F.E.M.A. Map No. 47037C0431F, (panel not printed)
- Any excavation, fill, or disturbance of the existing ground cover elevation must be done in accordance with Stormwater Management Ordinance 78-840 and approved by the Metro Department of Water Services.
- Metro Water Services shall be provided sufficient and unencumbered ingress and egress at all times in order to maintain, repair, replace, and inspect any stormwater facilities within the property.
- Size driveway culverts per the design criteria set forth by the Metro Stormwater Management Manual. (Minimum driveway culvert size in Metro ROW is 15")
- This drawing is for illustration purposes to indicate basic premise of the development. Final layout and details of the plan shall be governed by the appropriate regulations at the time of final application.
- The buffer along the waterways will be in an area where the surface is left in a natural state, and is not disturbed by construction activity. This in accordance with the Stormwater Management Manual Volume 1 - Regulations.
- Water Quality Treatment to be provided as required by Metro Water Services. Method of treatment to be provided with Final Site Development Plans.
- Detention to be provided for the site to maintain post-development flows to equal or less than pre-development flows for the 2yr - 100 yr storm events as required by Metro Water Services.
- Possible Waters of the State shown based on Metro Online GIS mapping and Regulatory Constraints Analysis done by BDY Natural Sciences Consultants.
- All construction shall be in accordance with the current standards and details of Metro Water Services - Stormwater Department and Metro Public Works.
- Metro Water Services Volume 5 Low Impact Development Stormwater Management Manual requirements will be met with this development.

## Water and Sewer General Notes

- All water and sewer construction will be in accordance with the current specifications and standard details of Metro Water Services at time of final development plans.
- A water and sewer service line will be provided for each townhome or single family unit.
- Multifamily units are to be served as required by Metro Water Services - Water & Sewer Department.