



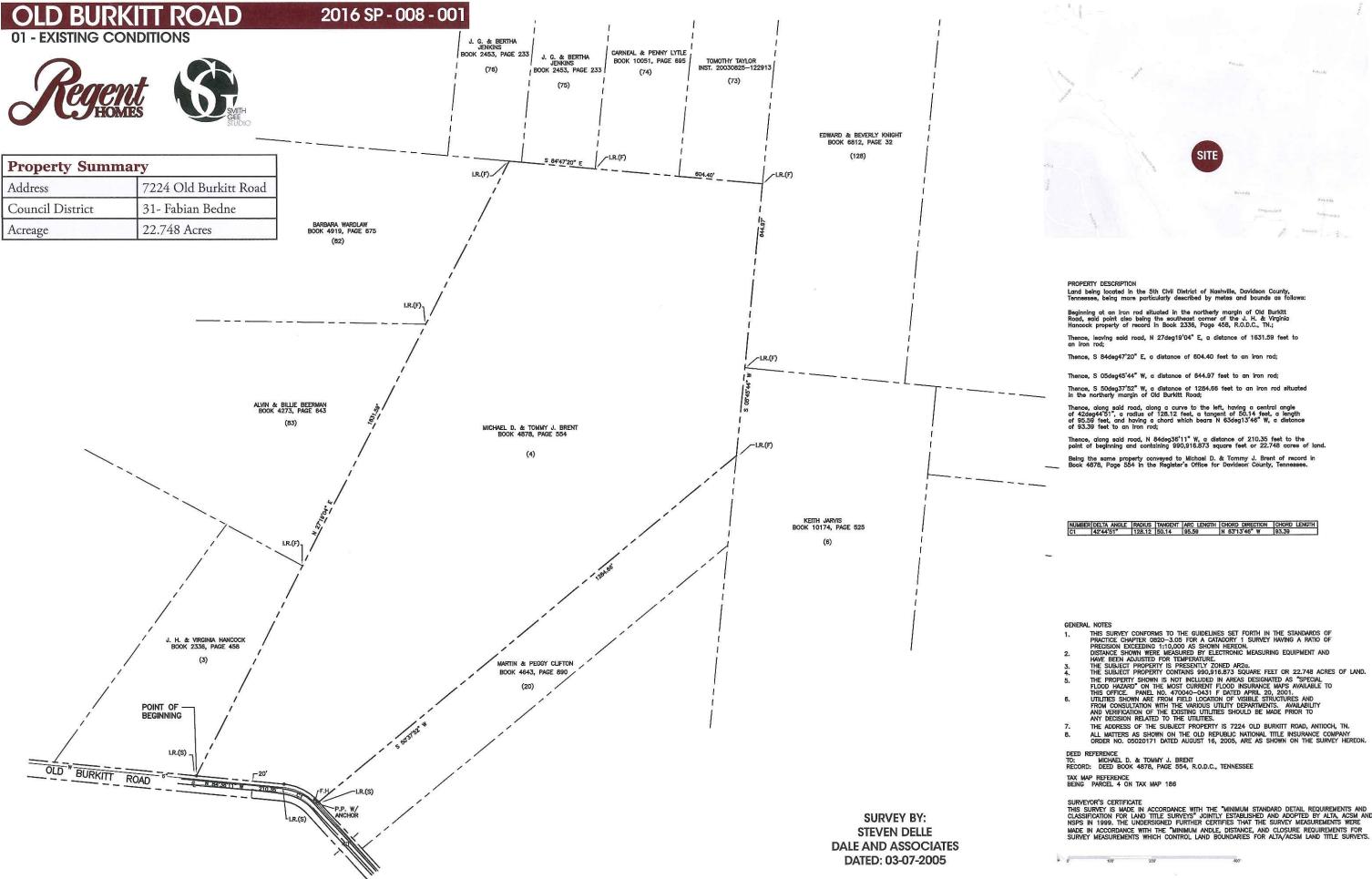


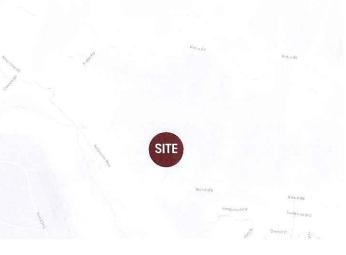
MPC Case Number - 2016 SP - 008 - 001 OLD BURKITT ROAD

7224 OLD BURKITT ROAD

Specific Plan

December 17 2015 (Revised: 3/1/2016)





Land being located in the 5th Civil District of Nashville, Davidson County, Tennessee, being more particularly described by metes and bounds as follows:

Beginning at an iron rod situated in the northerly margin of Old Burkitt Road, said point also being the southeast corner of the J. H. & Virginia Hancock property of record in Book 2336, Page 458, R.O.D.C., TN.;

Thence, S 84deg47'20" E, a distance of 604.40 feet to an iron rod;

Thence, S 05deg45'44" W, a distance of 644.97 feet to an iron rod;

Thence, \$ 50deg37'52" W, a distance of 1284,66 feet to an iron rod situated in the northerly margin of Old Burkitt Road;

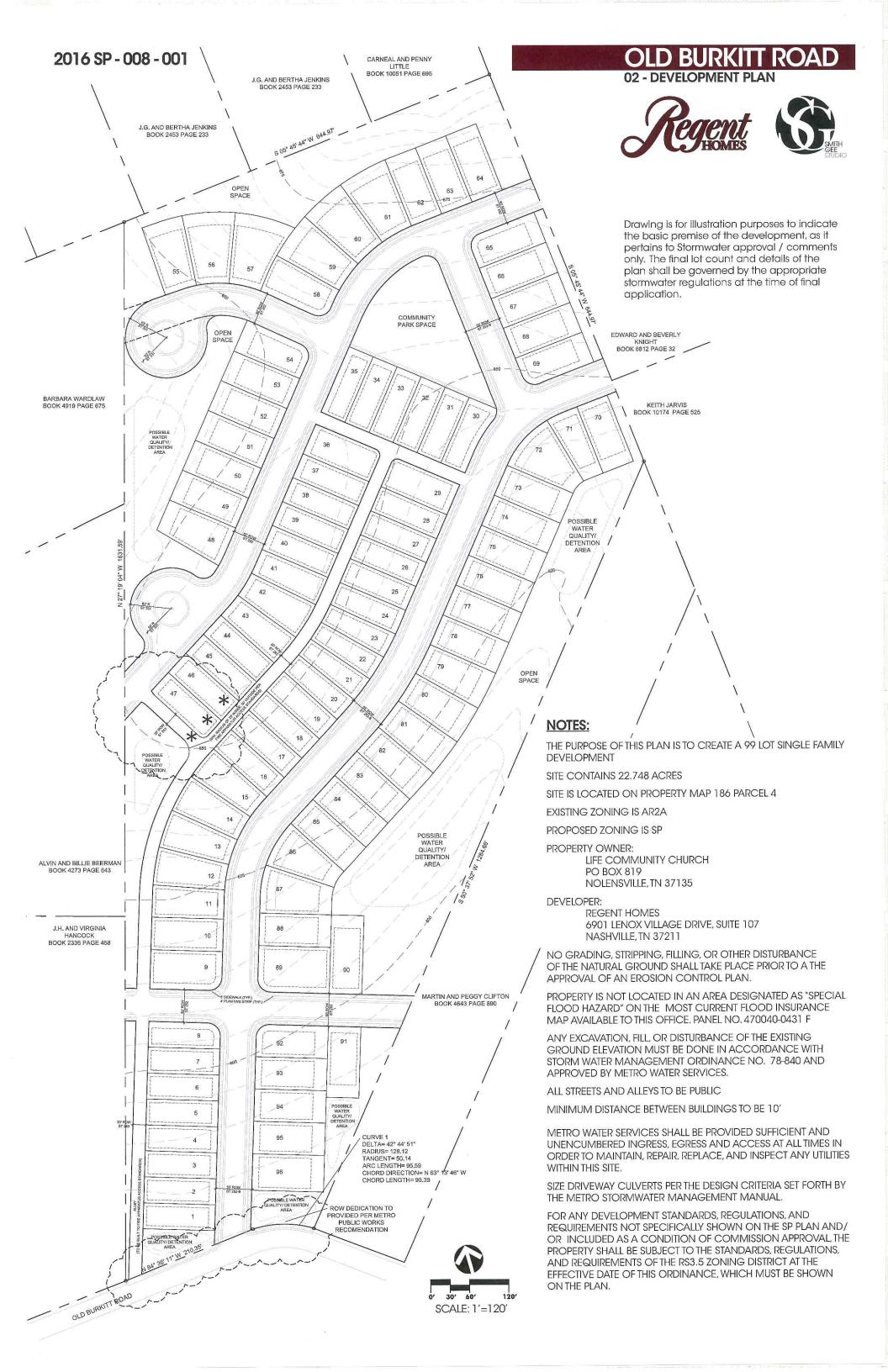
Thence, along said road, along a curve to the left, having a central angle of 42deg44*51*, a radius of 128.12 feet, a tangent of 50.14 feet, a length of 95.59 feet, and having a chord which bears N 63deg13*46* W, a distance of 93.39 feet to an iron rod;

Thence, along said road, N 84deg36'11" W, a distance of 210.35 feet to the point of beginning and containing 990,916.873 square feet or 22.748 acres of land.

 NUMBER DELTA ANGLE
 RADIUS
 TANGENT
 ARC LENGTH
 CHORD DIRECTION
 CHORD LENGTH

 C1
 42*44*51**
 128.12
 50.14
 95.59
 N 63*13*46** W
 93.39

SURVEYOR'S CERTIFICATE
THIS SURVEY IS MADE IN ACCORDANCE WITH THE "MINIMUM STANDARD DETAIL REQUIREMENTS AND
CLASSIFICATION FOR LAND TITLE SURVEYS" JOINTLY ESTABLISHED AND ADOPTED BY ALTA, ACSM AND
NSPS IN 1999. THE UNDERSIGNED PURTHER CERTIFIES THAT THE SURVEY MEASUREMENTS WERE
MADE IN ACCORDANCE WITH THE "MINIMUM ANDLE, DISTANCE, AND CLOSURE REQUIREMENTS FOR
SURVEY MEASUREMENTS WHICH CONTROL LAND BOUNDARIES FOR ALTA/ACSM LAND TITLE SURVEYS.



03 - DEVELOPMENT NOTES





Development Summary	
Uses	99 Single Family Residences
Open Space Area	5.5 Acres
Minumum Lot Width	40' (with alley) 50' (without alley)
Minumum Lot Depth	110'
Minumum Lot Area	4,400 sf. (with alley) 5,500 sf. (without alley)
Front Setback	10 ft.
Garage Setback	Street Loaded: 20 ft. Alley Loaded: Less than 5 ft. or greater than 15 ft.
Rear Setback	5 ft. (with alley) 20 ft. (without alley)
Side Setback	5 ft.
Height Standards	3 stories maximum in 40 feet*
Raised Foundation	18" Minimum, 5' Maximum (at Front Setback)
Access	1 Driveway cut per lot without an alley**
Density	4.88 du/ac

- * Measured from finished floor to top of ridgeline
- ** Lots with alley access MUST access from the alley and shall not have a driveway cut or vehicular access point from the street frontage.

Any standard not specifically addressed herein shall comply with the Residential Single Family 3,500 square foot lot (RS3.5) base zoning requirements as of the application date of this Preliminary SP.

Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.

Permitted Uses

All uses permitted by Residential Single Family 3,500 square foot lot (RS3.5) base zoning designation.

Parking

It is intended that the total parking provided will be in excess of the minimum requirements of the Metro Nashville Zoning Code parking standards.

Parking shall be provided within a parking pad and/or garage internal to the each lot.

On street parking will be provided within Public Works standard ST-252 B Right-Of-Ways.

Height

The maximum height permitted shall be three (3) stories within forty (40) feet measured from finished floor to top of ridgeline

Tree Conservation/Buffer

Where possible, large canopy trees and established tree groves will be preserved. A 10 open space buffer is required around the existing edges of the property.

General Plan Consistency

The specific plan proposed herein is located within the Southeast Community Plan. The specified land use is Neighborhood Evolving, Transect 3 (or T3 NE). The primary goal of T3 NE is to encourage denser housing types that are integrated into the fabric of the existing neighborhood. Neighborhood evolving encourages a diverse variety of housing types in interior neighborhoods.

This plan is consistent with the goals established throughout the policy and is also consistent with several critical planning goals. This plan creates a walkable neighborhood with various segments of open space that is directly accessible by 94% of all the proposed single family lots. Each property directly addresses the street and will be directly connected to a sidewalk lined with street trees.

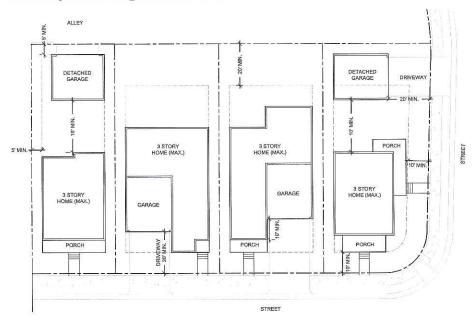
Architectural Standards

- Street facing building facades shall have a minimum of fifteen (15) percent glazing (measured as a percentage of each floors width x height) and have a minimum of one principal entrance (doorway).
- Homes along all street frontages shall have a porch or stoop with direct access to a public sidewalk.
- Windows shall be vertically oriented at a ratio of 1.5:1 or greater, excluding dormers.
- Refuse collection, recycling and mechanical equipment shall be fully screened from public view by the combination of fences, walls or landscaping.
- All residential units shall have a minimum of eighteen (18) inches and a maximum of five
 (5) feet raised foundations of at the street frontage, measured at the front setback. Minor reductions may be granted by Planning Staff with the Final SP application for topographically challenged areas and ADA required units.
- All ground supported porches shall provide a minimum of five (5) feet of depth. (expect for juliet balconies or like designs)
- EIFS, vinyl siding and untreated wood shall be prohibited (vinyl sofits shall be permitted).
- · Stairs and uncovered stoops may encroach up to 5' into the required front setback

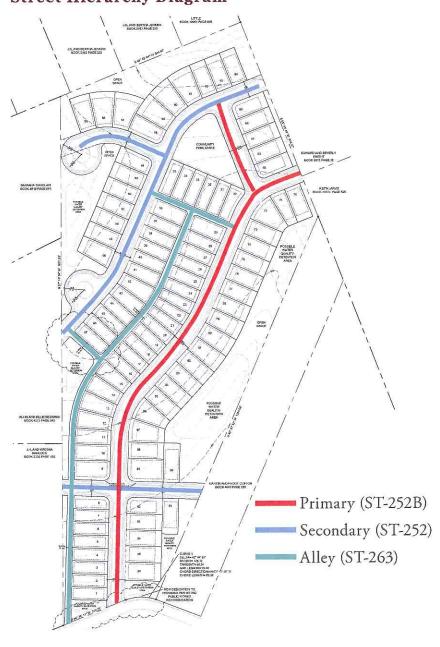
General Lot Layout Standards

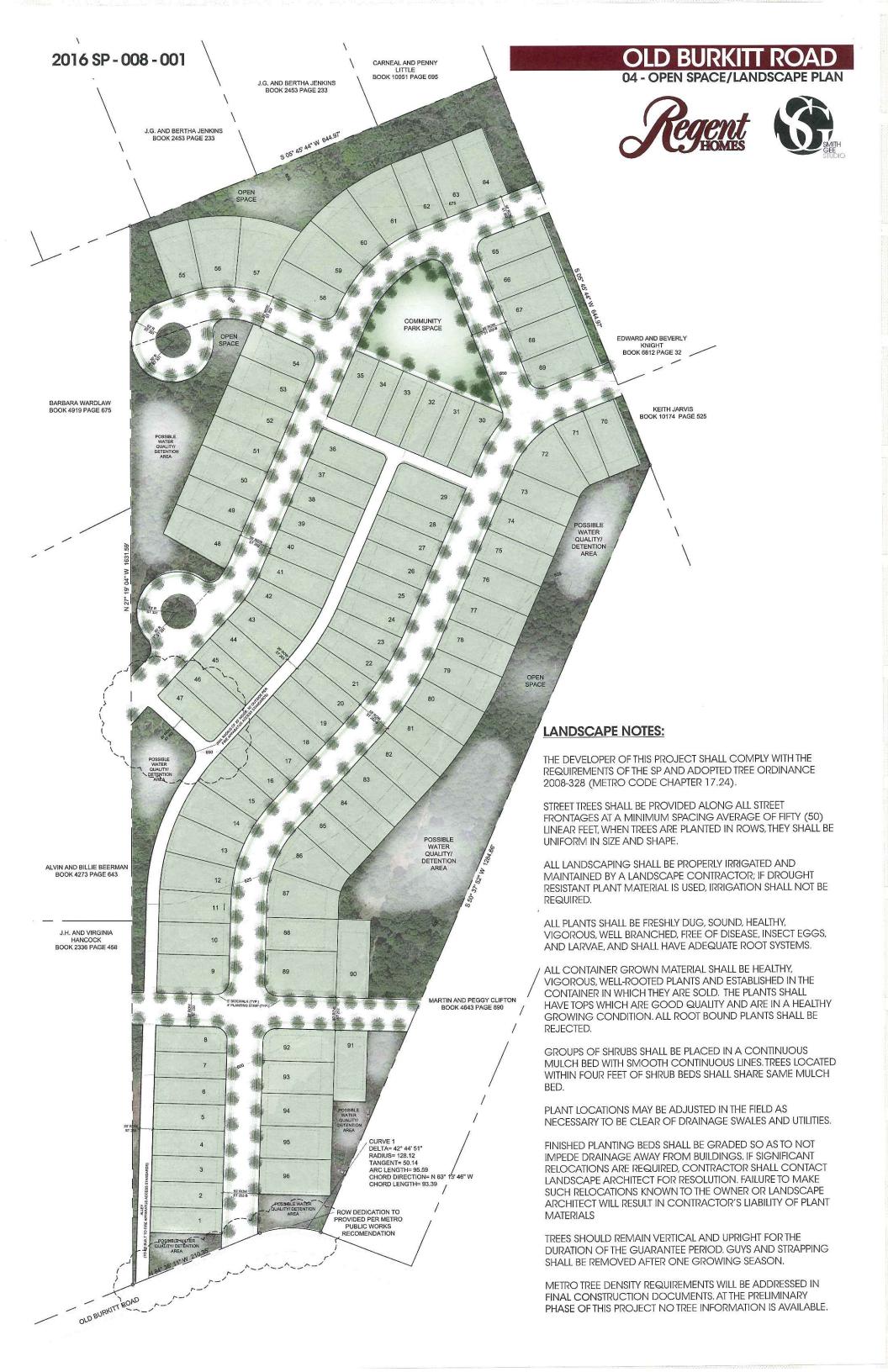
- All homes must present a front door facing the street frontage.
- Corner lots shall incorporate architectural details that address both street frontages.
- Corner lots must place garage access on the longest street frontage of the lot.
- Detached Garages shall be detached from the principle structure by 10' or greater.
- Detached Garages that are 700 square foot or smaller may encroach into the required rear setback up to 5' from the rear property line.
- All Garage doors must be setback at least 20' from the public sidewalk.
- Street fronting attached garages must be setback 10' from the leading edge of the building facade (excluding porch)

Lot Layout Diagram (NTS)



Street Hierarchy Diagram





OLD BURKITT ROAD

2016 SP - 008 - 001

05 - GENERAL NOTES



Erosion Control and Grading Notes

- Expose as small an area of soil as possible on the site for no more than 15 days. Keep dust within tolerable limits by sprinkling or other acceptable means.
- All cut/fill areas to have a minimum of 6" of topsoil cover. Areas dressed with topsoil shall receive 12pounds per 1000 square feet of 6-12-12 fertilizer (unless otherwise specified in written specifications), 5 pounds or more of Kentucky 31 fescue seed per 1000 square feet, and a straw mulch cover of 70%-80% coverage (approximately 125 pounds per 1000 square feet), unless otherwise noted within written specifications.
- Erosion control barrier is called out on plans and is to comply with the Metropolitan Stormwater Management Manual, volume four, section tcp-14.
- Disturbed areas are to be graded to drain as indicated in the plan to sediment barriers during and upon the completion of construction.
- The contractor shall be responsible for the verification and the location of any existing utilities. It shall be the responsibility of the contractor to avoid damage to all existing utilities during construction. If damage does occur to any such installation, full repair will be accomplished as per the current specification governing such work.
- Any access routes to the site shall be based with crushed stone, ASTM #1 stone, 100 feet long and at least 6" thick.
- The placing and spreading of any fill material is to be started at the lowest point and brought up in horizontal layers of 8" thickness (or as directed by the soils investigative report). Said fill material is to be free of sod, roots, frozen soils, or any other decomposable material. Said fill is to be compacted to a minimum of 95% standard proctor, or as otherwise specified by the soils report or written specifications.
- The contractor shall notify the Metro Davidson County Department of Public Works construction compliance division, three days prior to beginning the work.
- The contractor shall locate and stake the layout of the site in the field for inspection by the Architect. The contractor shall check the grades and final dimensions on the ground, and report any discrepancies to the Architect immediately for a decision.
- Surplus excavation of topsoil shall be placed on the site as approved by the owner for the purpose of future landscape use.
- The contractor shall furnish and install all necessary temporary works for the protection of the public and employees, including warning signs and lights.
- The contractor shall be responsible for any damage done to the premises or adjacent premises or injuries to the public during the construction caused by himself, his sub-contractors, or the carelessness of any of his employees.
- All work is to be completed with compliance to the rules and regulations set forth by Metro Water Services. The contractor shall give all necessary notice, obtain all permits, and pay fees required for the completion of his portion of the work. He shall also comply with all city, county and state laws and ordinance or regulations relating to portions of work which he is to perform.
- All erosion control measures shall remain in place until site is stabilized & construction is complete.
- Contractor shall provide an area for concrete wash down and equipment fueling in accordance with Metro CP-10 & CP-13. Location to be coordinated with the NP-DES department during the per-construction meeting.

Stormwater Notes

- The soil types for this area are Barfield-Rock outcrop complex, 5 to 20 percent slopes.
- This site shall comply as shown w/ Appendix 'H'/ the regulations residential infill guidance manual.
- Storm sewer system on this plan is shown schematically, final design will be provided during the final SP process and will meet the requirements of the stormwater management manual Appendix 'H'.
- The root crown to be at finished grade or no greater than a maximum of one inch higher (after settling) than finished grade.

Water & Stormwater Notes

- All water and sewer construction shall be in accordance with specifications and standard details of the Metro Water Services.
- The contractor is responsible for reimbursing the Metro Water Services the cost of inspection.
- The contractor is to provide and maintain the construction identification sign for private development approved.
- All connections to existing manholes shall be by coring and resilient connector method.
- Reduced pressure backflow prevention devices (rpbp) or dual check valve will be required on all test and fill lines (jumper) needed for water main construction and must be approved by the Metro Water Services.
- All water meters shall be a minimum of 24" not to exceed a maximum of 28" below finished grade.
- Pressure regulating devices will be required on the customer side of the meter when pressures exceed 100 psi.
- Pressure regulating devices will be required on the street side of the meter when pressures exceed 150 psi.

Utility Notes

- Water and Sewer service to be provided by Metro Water Services
- Final Water and Sewer service locations will be provided during the final SP process.

Public Works Notes

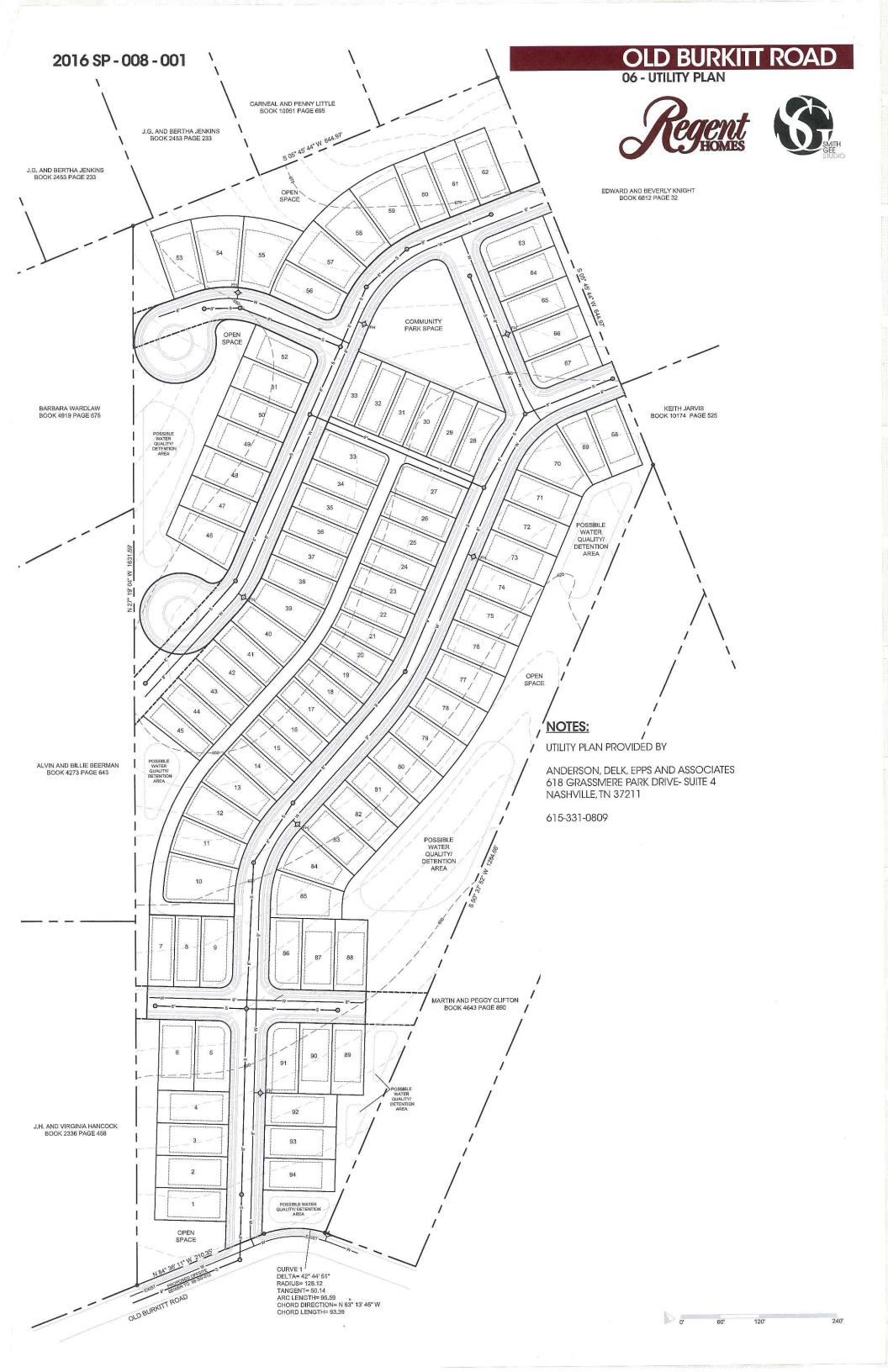
- All work within the public right of way requires an excavation permit from the Department of Public Works.
- Proof-rolling of all street subgrades is required in the presence of the public works inspector. Inspection of the binder course is required prior to final paving in the presence of the public works inspector. These requests are to be made 24 hours in advance.
- Stop signs are to be 30 inch by 30 inch.
- Street signs to have six inch white letters on a nine inch green aluminum blade, high intensity reflective.
- All pavement marking are to be thermoplastic.

Standard Notes

- All public sidewalks are to be constructed in conformance with Metro Public Works sidewalk design standards.
- Wheel chair accessible curb ramps, complying with applicable Metro Public Works standards, shall be constructed at street crossings.
- The required fire flow shall be determined by the Metropolitan Fire Marshal's Office, prior to the issuance of a building permit.
- Individual water and/or sanitary sewer service lines are required for each unit.

Development Schedule

• It is anticipated that the project will begin construction in August of 2016 and will take 12 months to be completed.







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NOTES:

THE PURPOSE OF THIS PLAN IS TO CREATE A 99 LOT SINGLE FAMILY DEVELOPMENT

SITE CONTAINS 22.748 ACRES

SITE IS LOCATED ON PROPERTY MAP 186 PARCEL 4 EXISTING ZONING IS AR2A

PROPOSED ZONING IS SP

PROPERTY OWNER:
LIFE COMMUNITY CHURCH
PO BOX 819

NOLENSVILLE, TN 37135
DEVELOPER:

REGENT HOMES
6901 LENOX VILLAGE DRIVE, SUITE 107

NASHVILLE, TN 37211

NO GRADING, STRIPPING, FILLING, OR OTHER DISTURBANCE OF THE NATURAL GROUND SHALL TAKE PLACE PRIOR TO A THE APPROVAL OF AN EROSION CONTROL PLAN.

PROPERTY IS NOT LOCATED IN AN AREA DESIGNATED AS

"SPECIAL FLOOD HAZARD" ON THE MOST CURRENT FLOOD INSURANCE MAP AVAILABLE TO THIS OFFICE. PANEL NO. 470040-0431 F

ANY EXCAVATION, FILL, OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH

STORM WATER MANAGEMENT ORDINANCE NO. 78-840 AND APPROVED BY METRO WATER SERVICES.
ALL STREETS AND ALLEYS TO BE PUBLIC

MINIMUM DISTANCE BETWEEN BUILDINGS TO BE 10'

METRO WATER SERVICES SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED INGRESS, EGRESS AND ACCESS AT ALL TIMES IN ORDER TO MAINTAIN, REPAIR, REPLACE, AND INSPECT ANY UTILITIES WITHIN THIS SITE.

SIZE DRIVEWAY CULVERTS PER THE DESIGN CRITERIA SET FORTH BY THE METRO STORMWATER MANAGEMENT MANUAL.

FOR ANY DEVELOPMENT STANDARDS, REGULATIONS, AND REQUIREMENTS NOT SPECIFICALLY SHOWN ON THE SP PLAN AND/OR INCLUDED AS A CONDITION OF COMMISSION APPROVAL, THE PROPERTY SHALL BE SUBJECT TO THE STANDARDS, REGULATIONS, AND REQUIREMENTS OF THE RS3.5 ZONING DISTRICT AT THE EFFECTIVE DATE OF THIS ORDINANCE, WHICH MUST BE SHOWN ON THE PLAN.

