



MPC Case Number - 2015 SP - 084 - 001

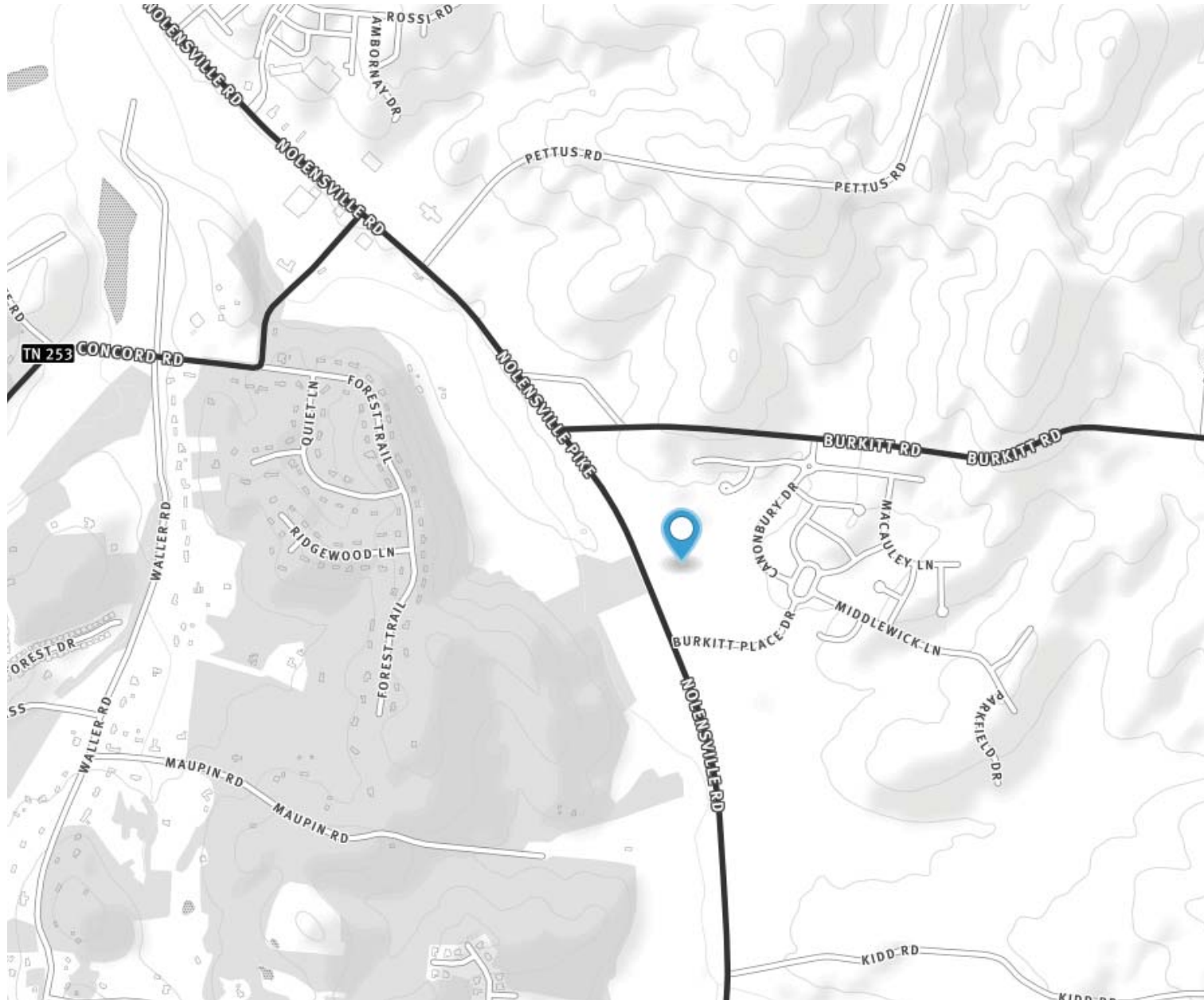
BURKITT COMMONS

BURKITT ROAD AT NOLENSVILLE PIKE

Specific Plan

October 20, 2015

Project Background



Site Info
 Site is 17.98 acres of land located at:
 7022 Nolensville Pike
 Map & Parcel(s): 18600001401,
 18600002600, 18600002100
 Council District 31 - Fabian Bedne

Owners of Record
 Newco-Burkitt, LLC
 9019 Overlook Blvd Ste C2
 Nashville, TN 37216

Developer
 PGM Properties
 9019 Overlook Boulevard - Suite
 C-2 Brentwood, TN 37027
 615-370-8381

Developer
 CPS Land, LLC
 213 Overlook Circle
 Brentwood, TN 37027
 615-376-5177

Civil Engineer
 4Site Incorporated
 256-539-1221
 jcargile@4siteinc.biz

Land Planner / Architect
 Smith Gee Studio
 615-645-5527
 bmiskelly@smithgeestudio.com

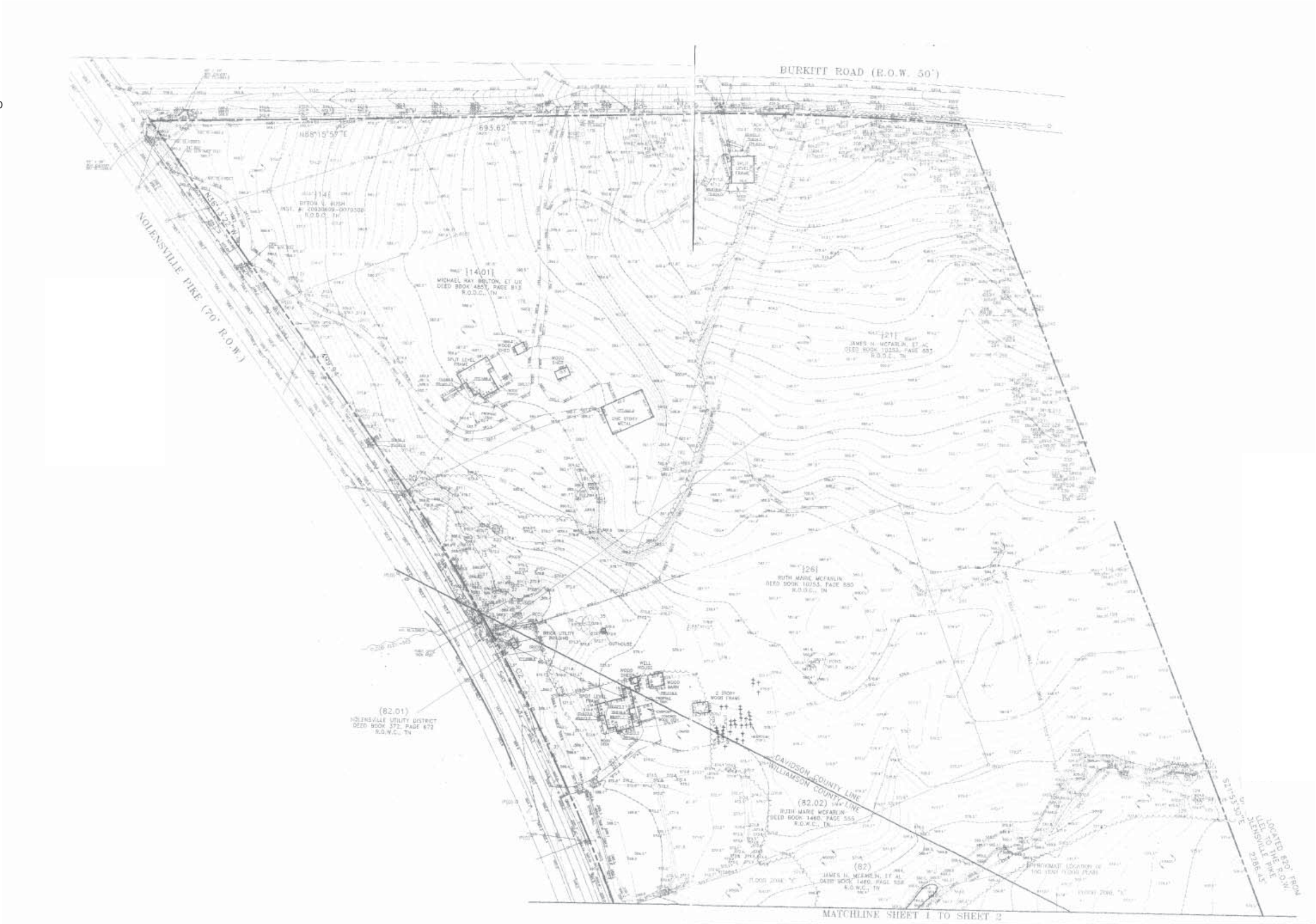


Existing Conditions

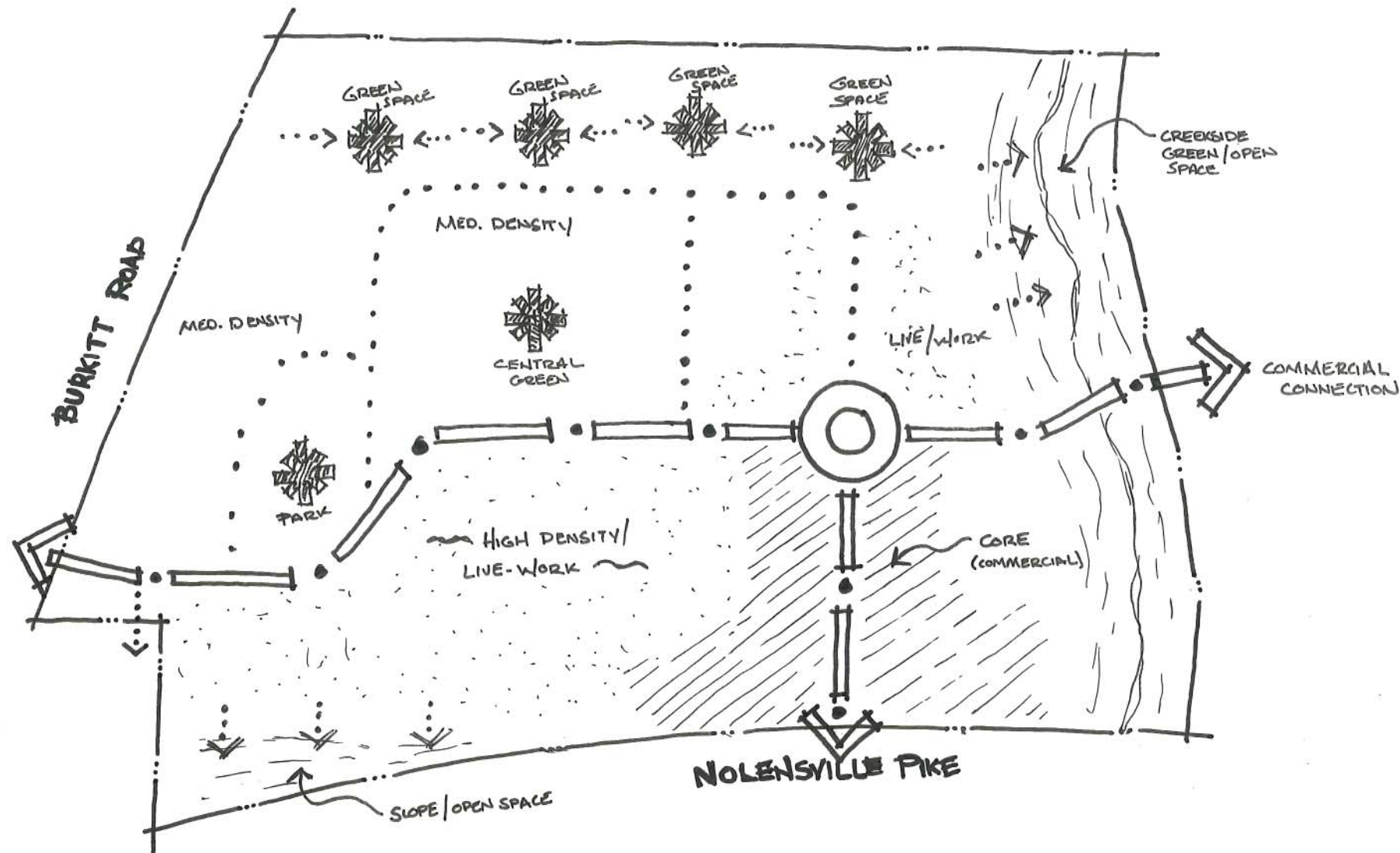
Site Info

Site is 17.98 acres of land located at:
7022 Nolensville Pike
Map & Parcel(s): 18600001401, 18600002600, 18600002100
Council District 31 - Fabian Bedne

Current Zoning: SP
Land Use Policy: T3 Community Center, Conservation



Site Concept



Village Concept

Burkitt Commons is designed with the following time-tested, traditional planning principles to provide a safe, integrated street network, neighborhood amenities and a sense of community.

- The interconnected street grid provides for dispersion of automobile traffic and multiple access points for emergency vehicles.
- Multiple entrances and future street extensions allow for dispersion of automobile traffic and potential connections for future development.
- Alleys and private autocourts provide a place for trash pickup, utilities and parking.
- The Village streets encourage residents to walk by providing sidewalks, street trees and traffic calming techniques such as narrow lanes and parallel parking.
- Both formal and informal open spaces provide a variety of active and passive recreation opportunities for the public and focal points for the community.
- The commercial/office core provides residents with access to neighborhood services without getting on Nolensville Pike.
- The core is situated in such a way that creates convenient automobile access for the entire community while allowing residents from the village and other adjacent neighborhoods to walk to neighborhood retail and services.
- Parking lots within the core occur behind the buildings, allowing for a pedestrian oriented streetscape.
- Diverse residential building types provide housing options with a range of affordability.
- Integrating housing typologies with compatible architectural design gives a sense of community to all residents.

Site Design



Design Plan

Through the Specific Plan, the community will be ensured a level of quality and a sense of community. The intent of the Burkitt Commons SP is to provide a comfortable, safe, mixed-use community with emphasis on pedestrian oriented streetscapes, diverse housing options, adequate open space and quality architecture.

More specifically, the SP and supporting Design Guidelines are intended to:

- Insure the compatible integration of retail, office and institutional uses with residential uses;
- Insure the compatible integration of a variety of housing types town-houses and multi-family, in order to accommodate the housing needs of a diverse population;
- Maintain a scale and form of development that emphasizes sensitivity to the pedestrian environment.
- Minimize the impact of automobiles into the setting through strategies such as "shared parking," in which adjacent land uses having different peak-hour parking demands can share parking facilities;
- Minimize the need for vehicles to travel on Nolensville Road, or to travel significant distances on Nolensville Road, by providing neighborhood commercial uses within close proximity to residents;
- Achieve "traffic calming" benefits through: an integrated street network providing options for traffic flow, the avoidance of excessively wide streets, and the provision of on-street parking.
- Provide for a variety of strategically-located and carefully-designed public and/or common spaces, including streets, greens and informal open space;
- Insure the compatibility of buildings with respect to the specific character of their immediate context within this SP boundary.
- Encourage active ground floor uses, such as restaurants, residential, shops and services, to animate the street within the core portion of the area.

Land Use Policy

Current Land Use Policy

The property is located within of the Southeast Community Plan. The current land use policy for the entire property is T3 Community Center and T3 Conservation. The SP site plan will retain the T3 CC and Conservation policies and create a solid transition from the commercial nature of Nolensville Pike to the adjacent neighborhood.

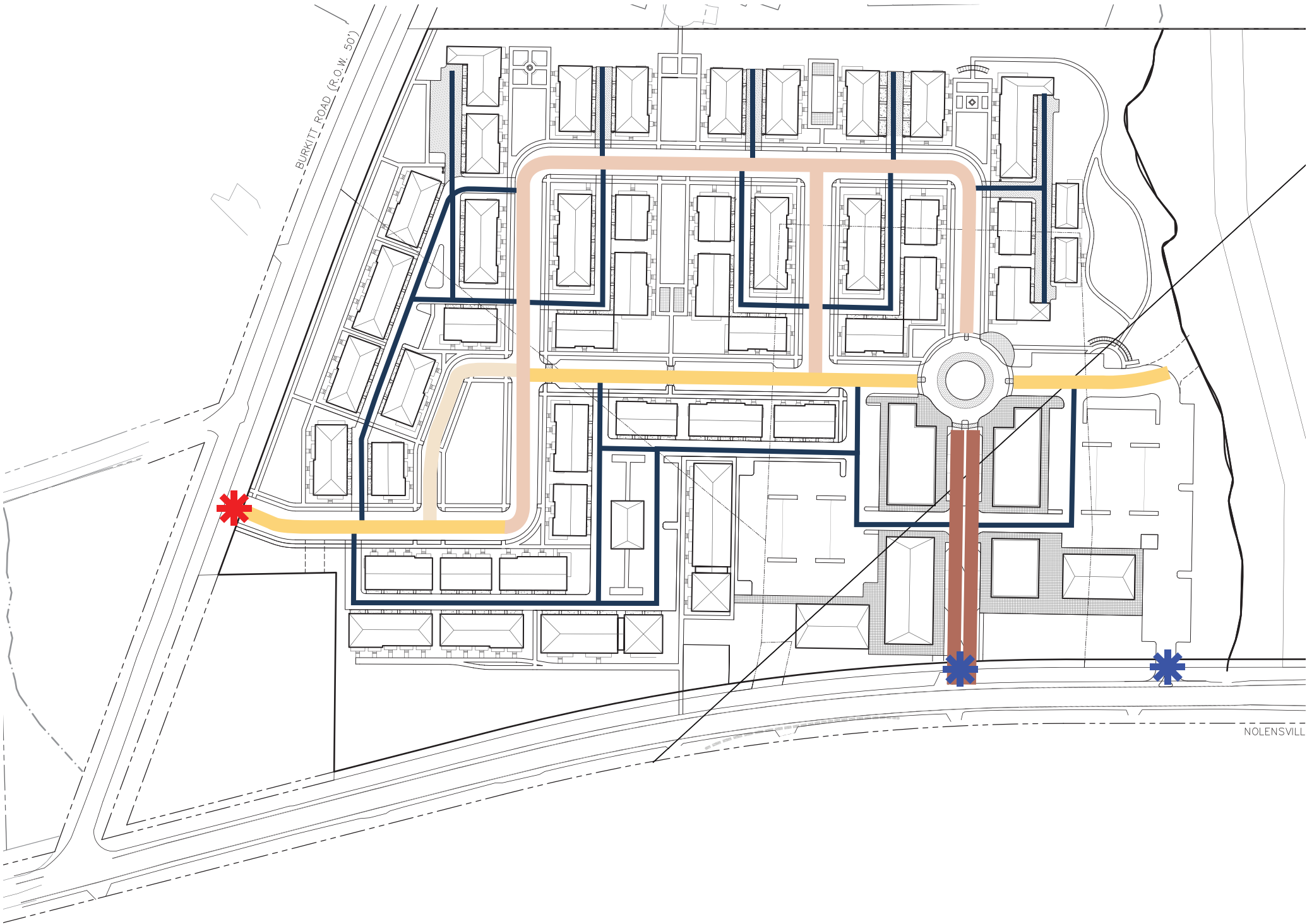
T3 Community Center

T3 Suburban Community Centers are pedestrian friendly areas, generally located at prominent intersections that contain mixed use, commercial, and civic and public benefit land uses, with transitional residential land uses in mixed use buildings or serving as a transition to adjoining Community Character Policies. The public realm and streetscape features the consistent use of lighting and formal landscaping. T3 Suburban Community Centers are served by highly connected street networks, sidewalks and mass transit leading to surrounding neighborhoods and open space. The edges of T3 Suburban Community Centers are firm with residential transitions between the center and less intense suburban residential and open space areas, with distinguishable boundaries identified by land uses, building types, building placement, and block structure.

T3 Conservation

Conservation Community Character policy is found in all Tract Categories except T6 Downtown. Its intent is to preserve environmentally sensitive land features through protection and remediation. Environmentally sensitive land features are kept in a natural state and any development is minimal to protect water quality, minimize infrastructure and public service costs, and preserve the unique environmental diversity








Street Network



Street Network Design

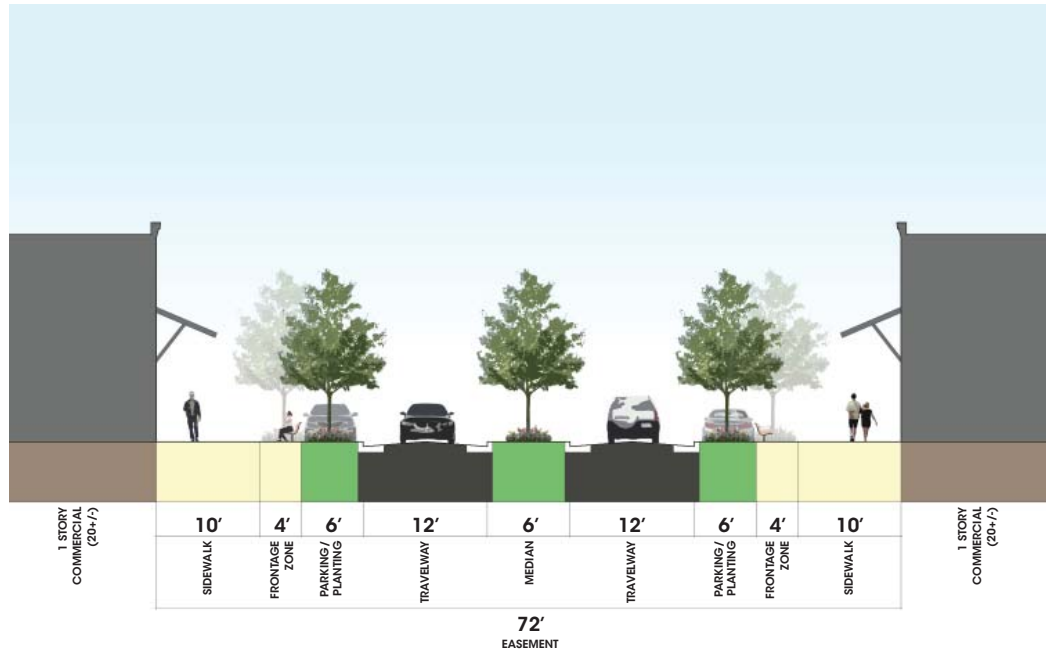
The Burkitt Commons interconnected street network is designed to disperse automobile traffic while providing multiple entrances and access points for emergency vehicles (with one (1) connection within Davidson County and two (2) access points within Nolensville). A total of three (3) access points from outside the site are proposed. Future street extensions are desired to further disperse traffic and to provide connections to the SP from future developments. All streets and alleys within the development are intended to be constructed as private infrastructure with public easements for access and utilities. The streets and alleys will be owned and maintained by a private organization made up of property owners.

Legend

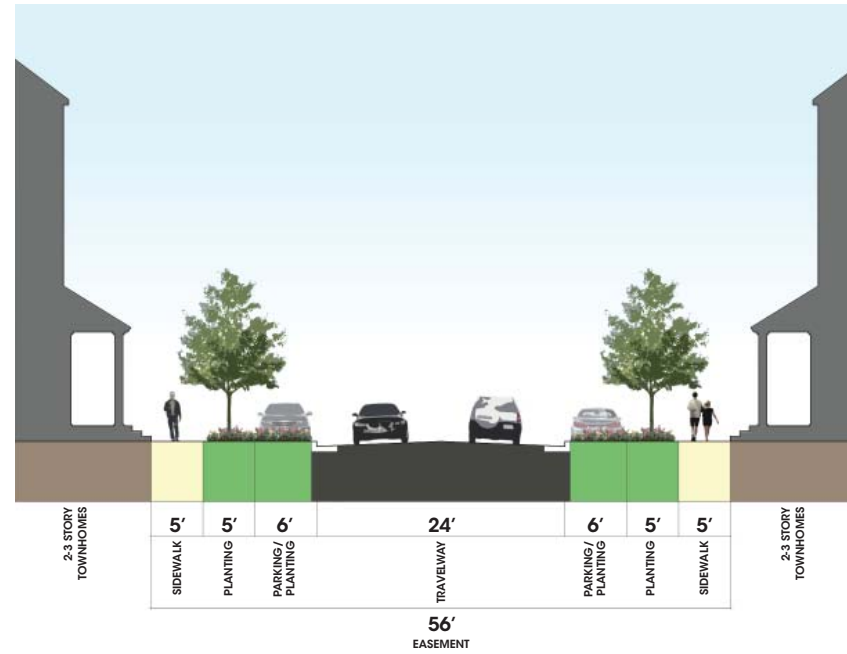
-  Town Boulevard- 72' Easement
-  Center Street- 56' Easement
-  Neighborhood Street- 54' Easement
-  Park Street- 40' Easement
-  Alley- 20' Easement
-  Davidson County Vehicular Access
-  Nolensville Vehicular Access

Street Network

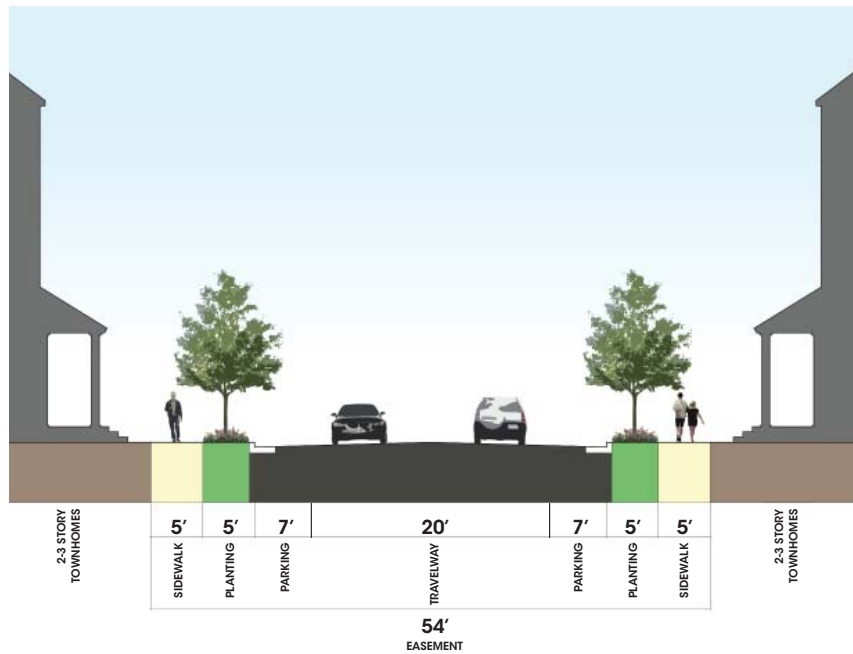
Town Boulevard



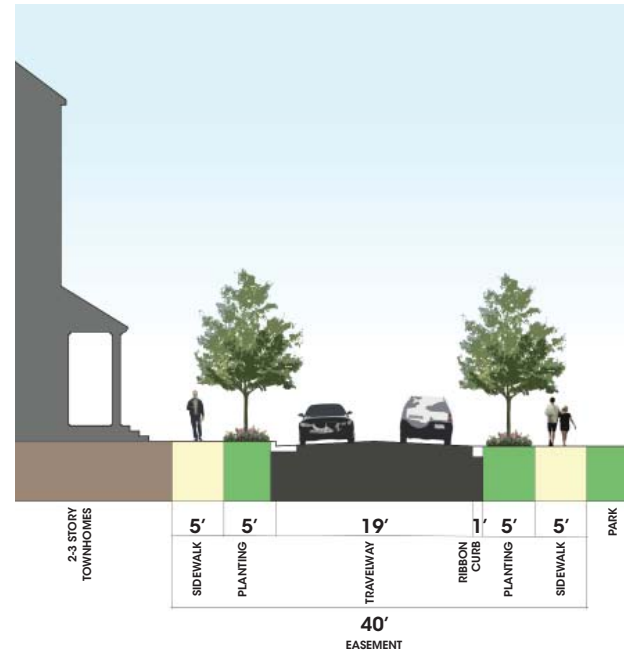
Center Street



Neighborhood Street



Park Street



Street Sections

Each street type in Burkitt Commons is designed for specific conditions based on projected frequency of traffic, desired parking conditions, the types of uses and intensity of housing along that street, specific physical conditions and to allow for emergency vehicle access. While the Street Network/Street Sections illustrate the desired street layout and design, the plan shall be flexible to respond to physical site conditions, dispersion of building types, community desires and a changing market. The SP shall allow for variations in the design of the street network, street sections and block layout so long as it meets the intent of the regulations and are approved by Metro Public Works.

All roadways shall be built with Metro Public Works standard ST-200 curb and gutters and Metro Public Works standard ST-210 sidewalks.

Each street intersection shall be designed and engineered specifically to allow for a 39' x 9' fire engine/school bus to make all possible turns free of parked cars and curbs. Final construction plans will provide turning diagrams ensuring adequate turning movements.

Note: All public streets/drives within the SP boundary will have a design speed and posted speed of 20 miles per hour

Nolensville Road

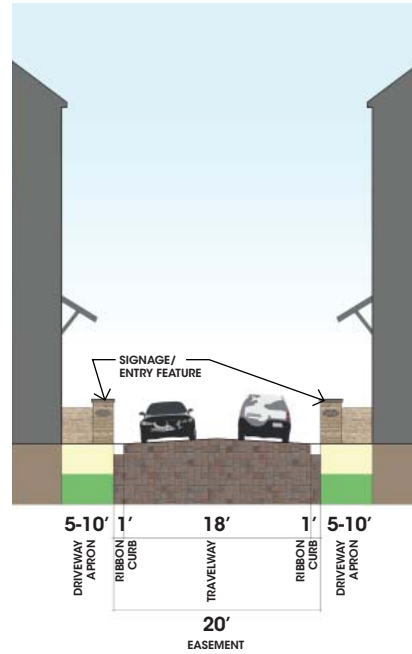
Project team will work with TDOT and Metro Nashville to appropriate R.O.W dedications for planned improvements of Nolensville and Burkitt Roads.

Standard Notes

- Roadway Improvements that are a direct result of this specific project as determined by the approved Traffic Impact Study and the Department of Public Works shall be constructed.
- Any required right-of-way within the project site that is identified as necessary to meet the adopted roadway plans shall be dedicated.
- In compliance with the Major and Collector Street Plan requirements, a 6 foot planting strip and 8 foot sidewalk shall be provided along Nolensville Pike and Burkitt Road. We shall coordinate with Public Works and TDOT to ensure the location is consistent with plans to widen Nolensville Pike.

Street Network

Alley

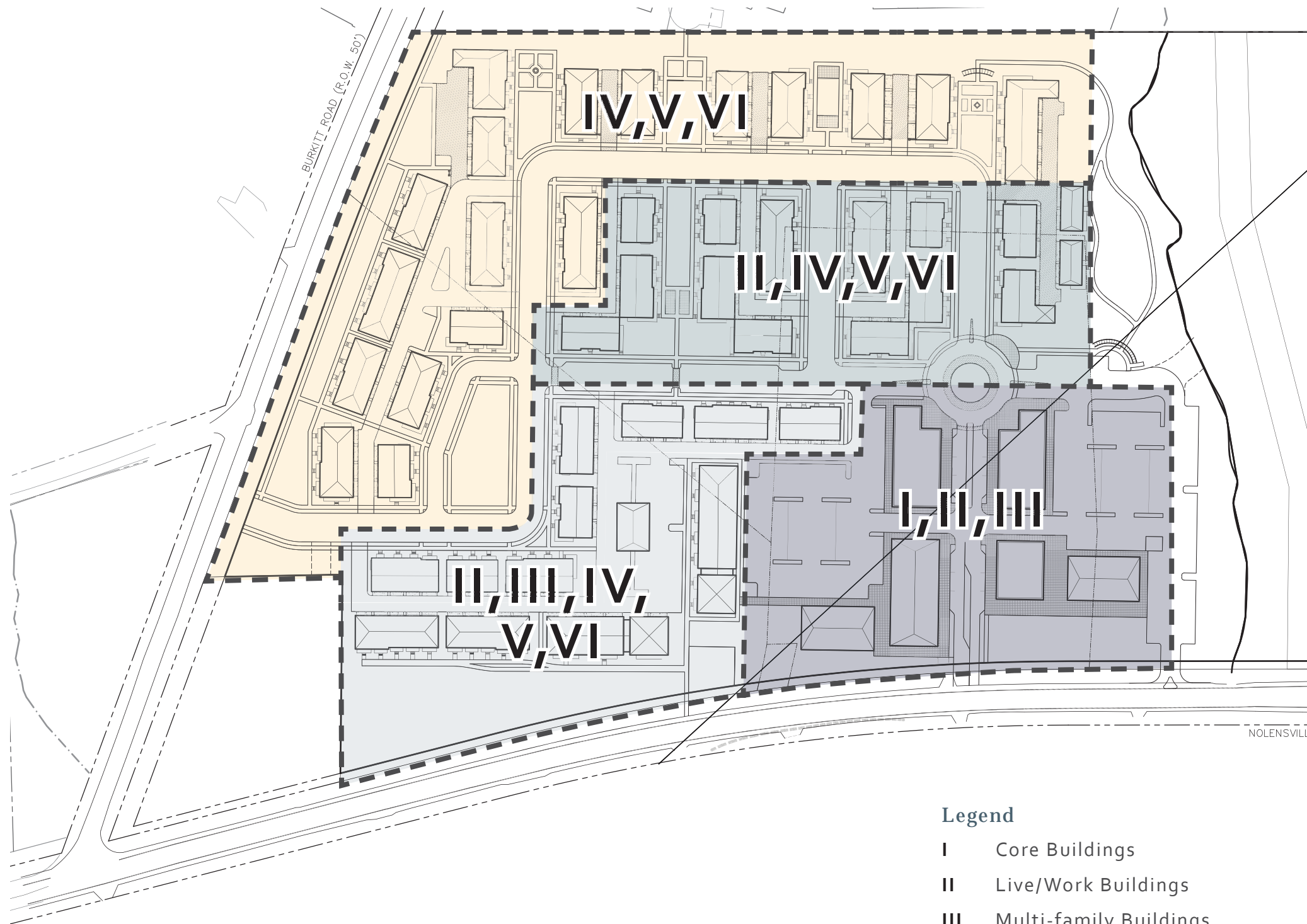


Alleys

All garage units are accessed via an alley. The intent is to limit the amount of direct access from the street network allowing better traffic flow across the system.

Alleys are intended to circumnavigate throughout the site to make accessing units easier from each street and to conceal parking and utilities from public view.

Building Typology



Building Typology Plan

Burkitt Commons is designed as a mixed use community, integrating diverse housing types, retail/restaurant, office and community services.

The core is intended to accommodate denser residential office and retail uses. Denser multifamily housing shall be limited to around the core and along Nolensville, however this multifamily shall not feel disconnected from the remainder of the community. Townhouses shall be integrated throughout the community.

See following pages for regulations and design guidelines associated with each building typology. Bulk standards provided shall supersede the corresponding base zoning bulk standards.

Block Layout

The following principles are to be followed in the layout and distribution of the building typologies and parcels:

- At tee intersections, where possible streets shall terminate on axis with a primary building form or architectural feature or on open space.
- On corner lots, architectural features must address both the front and side streets (e.g. corner porches, side porches, bay windows, etc.) and blank side walls shall be avoided.
- Townhouses should not be located to face directly down an alley.
- Common pedestrian passages shall be provided between parcels to allow exterior access from front to back of townhouses at reasonable intervals.
- Detached garages shall be limited to internal parking lot areas or as an accessory use located on an alley. Detached garages are subject to the bulk standards of the primary use if used as an accessory structure.
- Carriage units shall be limited to internal parking lot areas or adjacent to an alley or passive open space; carriage units shall not have direct frontage on street frontages or formal open space.

Legend

- I Core Buildings
- II Live/Work Buildings
- III Multi-family Buildings
- IV Townhomes
- V Carriage Units
- VI Detached Garages

Building Typology



Core Buildings (Type I)

Intent: Village Core Buildings are intended to serve the neighborhood with retail, restaurant, office and residential uses. Ground floors have store-fronts along street facades.

Front Build-to Line: Where buildings directly front public streets or common areas (e.g. sidewalks, parking, open space), 80% of the front facade must be built to the back of the proposed sidewalk. Projections and recesses up to 2 feet for architectural articulation are permitted.

Note: For buildings fronting Nolensville Pike, 100% of the front building facade shall be built within +/- 20 feet to the building locations shown on the approved SP site plan.

Minimum Building Separation: 10 feet

Maximum Height: 1 story within 25 feet. Building Height shall be measured from the FFE to the roof line.

Note: Architectural elements such as corner entries, turrets, towers, etc. measuring no more than 25 feet in width (per frontage) may extend up to 35 feet in height (vertically).

Minimum Ground Floor Height: 14 feet. Height shall be measured from the FFE to the ceiling.

Primary entrance / Glazing : Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing for residential and 40% glazing for office. (calculated as a % of ground floor facade measured from finished floors to ceiling)

Off Street Parking Spaces: Per Metro Zoning Code

Off Street Parking Location: Where provided, off street parking shall be located behind buildings.

Passages: Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

Rear Setback: 10 feet

Side Setback: 5 feet

SEE REGULATIONS SHEET FOR ADDITIONAL DESIGN STANDARDS

Building Typology



Live/Work Buildings (Type II)

Intent: Live/Work Buildings are intended to provide the opportunity to live directly above a workplace. Ground floors have retail, office or residential uses.

Front Build-to Line: Where buildings directly front public streets or common areas (e.g. sidewalks, parking, open space), 80% of the front facade must be built to within 0-10 feet from the back of the proposed sidewalk. Projections and recesses up to 2 feet for architectural articulation are permitted.

Note: For buildings fronting Nolensville Pike, 100% of the front building facade shall be built within +/- 20 feet to the building locations shown on the approved SP site plan.

Minimum Building Separation: 10 feet.

Minimum Alley Setback: 5 feet or greater than 15 feet from edge of pavement.

Maximum Height: 3 1/2 stories in 52 feet. Building Height shall be measured from the average grade to the roof line.

Primary entrance / Glazing : Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing for residential and 40% glazing for office. (calculated as a % of ground floor facade measured from finished floors to ceiling)

Off Street Parking Spaces: Per Metro Zoning Code

Off Street Parking Location: Where provided, off street parking shall be located behind buildings with the exception of single loaded access drives.

Passages: Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

Rear Setback: 10 feet.

Side Setback: 5 feet (End units)

SEE REGULATIONS SHEET FOR ADDITIONAL DESIGN STANDARDS

Building Typology



Multi-family Buildings (Type III)

Intent: Multi-Family buildings are intended to provide an opportunity for a more inclusive community. Regulations and guidelines for Multi-Family buildings include clubhouses and accessory structures.

Front Build-to Line: Where buildings directly front public streets or common areas (e.g. sidewalks, parking, open space), 80% of the front facade must be built to within 0-15 ft from the back of the proposed sidewalk. Projections and recesses up to 2 feet for architectural articulation are permitted.

Note: For buildings fronting Nolensville Pike, 100% of the front building facade shall be built within +/- 20 feet to the building locations shown on the approved SP site plan.

Minimum Building Separation: 10 ft.

Minimum Alley Setback: 5 ft or greater than 15 ft from edge of pavement.

Maximum Height: 4 stories in 64 feet. Building Height shall be measured from the average grade to the roof line.

Minimum Height: 2 stories

Primary entrance / Glazing : Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing for residential and 40% glazing for office. (calculated as a % of ground floor facade measured from finished floors to ceiling)

Foundation Height: A raised foundation of 18 inches - 36 inches is required for all residential structures, with allowances for up to 60 inches for specific issues with topography or ADA required units.

Off Street Parking Spaces: Per Metro Zoning Code

Off Street Parking Location: Where provided, off street parking shall be located behind buildings with the exception of single loaded access drives.

Passages: Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

Rear Setback: 10 ft.

Side Setback: 5 ft.

SEE REGULATIONS SHEET FOR ADDITIONAL DESIGN STANDARDS

Building Typology



Townhomes (Type IV)

Front Build-to Line: Where buildings directly front public streets or common areas (e.g. sidewalks, parking, open space), 80% of the front facade must be built to within 5-15 feet from the back of the proposed sidewalk. Projections and recesses up to 2 feet for architectural articulation are permitted.

Note: For buildings fronting Nolensville Pike, 100% of the front building facade shall be built within +/- 20 feet to the building locations shown on the approved SP site plan.

Note: For buildings fronting Burkitt Road, 100% of the front building facade shall be built within +/- 20 feet to the building locations shown on the approved SP site plan.

Minimum Building Separation: 10 feet

Minimum Alley Setback: 5 feet or greater than 15 feet from edge of pavement.

Maximum Height: 3 1/2 stories in 52 feet. Building Height shall be measured from the average grade to the roof line.

Minimum Height: 2 stories

Foundation Height: A raised foundation of 18 inches - 36 inches is required for all residential structures, with allowances for up to 60 inches for specific issues with topography or ADA required units.

Primary entrance / Glazing : Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing for residential and 40% glazing for office. (calculated as a % of ground floor facade measured from finished floors to ceiling)

Off Street Parking Spaces: Per Metro Zoning Code

Off Street Parking Location: Where provided, off street parking shall be located behind buildings with the exception of single loaded access drives.

Passages: Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

Rear Setback: 10 feet.

Side Setback: 5 feet (End units)

SEE REGULATIONS SHEET FOR ADDITIONAL DESIGN STANDARDS

Building Typology



Carriage Units (Type V)

Minimum Width: 16 feet

Minimum Depth: 10 feet

Front Build-to Line: None

Note: Carriage units must have a face onto a green, open space or common area.

Minimum Building Separation: 10 feet (when detached)

Minimum Alley Setback: 5 feet or greater than 15 feet from edge of pavement.

Maximum Height: 3 stories in 42 feet. Building Height shall be measured from the average grade to the roof line.

Foundation Height: A raised foundation of 18 inches - 36 inches is required for all residential structures, with allowances for up to 60 inches for specific issues with topography or ADA required units.

Primary entrance / Glazing : Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of 15% glazing for residential and 40% glazing for office. (calculated as a % of ground floor facade measured from finished floors to ceiling)

Off Street Parking Spaces: Per Metro Zoning Code

Off Street Parking Location: Off street parking and garage access shall be from an alley or lane.

Passages: Landscaped passages are encouraged between buildings to provide access from rear parking areas to the building fronts.

Rear Setback: 10 feet

Side Setback: 5 feet

Carriage units shall be limited to internal parking lot areas or adjacent to an alley or passive open space; carriage units shall not have direct frontage on street frontages or formal open space.

SEE REGULATIONS SHEET FOR ADDITIONAL DESIGN STANDARDS

Building Typology



Detached Garages (Type V)

Front Build-to Line: None

Minimum Side Yard Setback: 3 ft.

Minimum Rear Yard Setback: 3 ft.

Maximum Height: 2 stories within 25 feet. Building Height shall be measured from the average grade to the roof line.

Detached garages shall be limited to internal parking lot areas or as an accessory use located on an alley. Detached garages are subject to the bulk standards of the primary use if used as an accessory structure.

SEE REGULATIONS SHEET FOR ADDITIONAL DESIGN STANDARDS

Landscape Plan



Landscape Standards

The developer of this project shall comply with the requirements of the SP and adopted tree ordinance 2008-328 (Metro Code Chapter 17.24).

Street trees shall be provided along all street frontages at a minimum spacing average of fifty (50) linear feet.

All landscaping shall be properly irrigated and maintained by a landscape contractor; if drought resistant plant material is used, irrigation shall not be required.

Where trees are planted in rows, they shall be uniform in size and shape.

All plants shall be freshly dug, sound, healthy, vigorous, well branched, free of disease, insect eggs, and larvae, and shall have adequate root systems.

All container grown material shall be healthy, vigorous, well-rooted plants and established in the container in which they are sold. The plants shall have tops which are good quality and are in a healthy growing condition. All root bound plants shall be rejected.

Groups of shrubs shall be in a continuous mulch bed with smooth continuous lines. All mulched bed edges shall be curvilinear in shape following the contour of the plant mass. Trees located within four feet of shrub beds shall share same mulch bed.

Plant locations may be adjusted in the field as necessary to be clear of drainage swales and utilities. Finished planting beds shall be graded so as to not impede drainage away from buildings. If significant relocations are required, contractor shall contact landscape architect for resolution. Failure to make such relocations known to the owner or landscape architect will result in contractor's liability of plant materials.

Trees must remain vertical and upright for the duration of the guarantee period. Guys and strapping shall be removed after one growing season.

The root crown to be at finished grade or no greater than a maximum of one inch higher (after settling) than finished grade.

A 25 foot "NO DISTURB" zone shall be provided on the Eastern property boundary to separate and buffer the development from the Burkitt Place community. The protection of trees shall follow the requirements of the zoning code 17.24.110. A construction fence shall be placed along this buffer prior to any site grading. Signage shall be provided every 100 ft. along the fence that says "Tree Protection Area - Do not Disturb". If the area is disturbed for any reason, impacted trees will be replaced per the zoning requirements for the replacement of trees 17.24.100.

Regulations

Zoning

For any development standards, regulations and requirements not specifically shown on the SP plan and /or included as a condition of Council approval, the property shall be subject to the standards, regulations and requirements of the MUL-A zoning district as of the date of the application request or application.

Minor modifications to the preliminary SP plan may be approved by the Planning Commission or its designee based upon final architectural, engineering or site design and actual site conditions. All modifications shall be consistent with the principles and further the objectives of the approved plan. Modifications shall not be permitted, except through an ordinance approved by Metro Council that increase the permitted density or floor area, add uses not otherwise permitted, eliminate specific conditions or requirements contained in the plan as adopted through this enacting ordinance, or add vehicular access points not currently present or approved.

Permitted Uses

All uses permitted by the Mixed-Use Limited Alternative (MUL-A) designation.

Within Davidson County, permitted land uses shall be limited to the following:

Nonresidential: 10,000 SF Max.

Residential: 200 units Max.

Parking

Provided parking shall meet the requirements of the parking standards of the Zoning Code (17.20).

It is intended that the total parking provided will be in excess of the minimum requirements of the Metro Zoning Code parking standards.

Parking shall be provided within parking areas and private garages internal to the development. Approximately 130 guest parking spaces shall be provided in the form of on-street parking throughout the SP.

Architectural Standards

Buildings shall avoid continuous uninterrupted blank facades. At a minimum, the facade plane shall be interrupted by one of the following for every thirty (30) linear feet of street frontage:

- A change in building material
- A horizontal undulation in the building facade of three (3) feet or greater
- A porch, stoop or balcony; porches shall be a min. six (6) feet in depth

Refuse collection, recycling and mechanical equipment shall be fully screened from public view by the combination of fences, walls or landscaping.

Building facades fronting a street shall provide a minimum of one principal entrance (doorway) and a minimum of fifteen (15)% glazing for residential and forty (40)% glazing for office. (calculated as a % of ground floor facade measured from finished floors to ceiling)

Windows shall be vertically oriented at a ratio of 1.5:1 or greater; Planning staff may allow modifications to this standard for dormers, decorative windows, clerestory windows, egress windows and other special conditions.

All ground level porches shall provide a minimum of six (6) feet of depth.

EIFS, vinyl siding and untreated wood shall be prohibited (vinyl soffits shall be permitted).

Signage

The following sign types shall be permitted: Projecting Signs, Wall Mounted Signs, Awning Signs, Ground Mounted Signs, Canopy Signs.

Ground mounted signs shall be limited to twelve (12) feet in width and six (6) feet vertically or signage area.

Signs may additionally include a maximum of a two (2) feet masonry foundation.

External Illumination: Light sources shall be directed towards sign minimizing glare to the sidewalk or adjacent properties.

Internal Illumination: Signs are permitted to be internally illuminated but the sign background must be opaque with only graphic, text or logos being illuminated.

Signs standards not addressed herein must meet the requirements permitted by the Mixed-Use Limited Alternative (MUL-A) base zoning designation.

Standard Notes

- The purpose of this SP is to receive preliminary approval to permit the development of a seventeen (17)+ acre mixed-use development as shown.
- HVAC units shall be located at the rear half of the side of unit, behind the unit, or on the roof of each building.
- Approval of this SP applies to the Davidson County portion of the project only.
- Roadway Improvements that are a direct result of this specific project as determined by the approved Traffic Impact Study and the Department of Public Works shall be constructed.
- Any required right-of-way within the project site that is identified as necessary to meet the adopted roadway plans shall be dedicated.